

Track Access Program Policy



June 2021

Version 4.0





Standard Operating Procedure ID #

200.1

Track Access Program

Original Date	January 9, 2018	Revision No.	4
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Track Access Program: The process governing access to the streetcar operational right-of-way, including the management, scheduling, and allocation authorization for access by Permittees to perform work or conduct a special event on, near, or adjacent to the right of way or any facilities associated with the El Paso Streetcar.

The Permittee must comply with all applicable federal, state, local laws, regulations, and standards affecting its work or event.

Policy

1. Purpose

- 1.1 The purpose of this policy is to maintain a safe environment and efficient transit system for customers, employees, contractors, and Permittees when an activity is being performed on the El Paso Streetcar (EPSC) Operational Right-of-Way (ROW), under any streetcar infrastructure or within 10 feet of any streetcar infrastructure.
- 1.2 For the purpose of this document, the following terms apply: City of El Paso is referred to as "COEP". Other parties, contractors, city departments, etc. are referred to as "Permittee(s)." The designated contact persons for EPSC are the Chief Safety Officer, the Streetcar Operations Superintendent, and the Right-of-Way (ROW) Coordinator.
- 1.3 The City of El Paso (COEP) has been authorized to construct and operate a fixed rail guideway system on COEP property.
- 1.4 The EPSC Operational ROW is the area including all tracks (single and double) and the space extending ten (10) feet from centerline of the track (Dynamic Safety Envelope), any portion of a station stop, and ten (10) feet from the Overhead Contact System (OCS). The EPSC Operational ROW also includes the substations, poles and wires that make up the OCS.
- 1.5 In order to maintain a safe environment for anyone requiring track access, the following policy and requirements must be adhered to by the applicant or city department requesting access to the EPSC Operational ROW and/or when an activity has the potential to affect the operation of El Paso Streetcar.



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- 1.6 Transit Supervisory Personnel, Operators, Contractors, Contractors' personnel, and Permittees working in or on the EPSC Operational ROW shall have a valid Track Access Training Card (TATC) in their possession. It is essential that all Permittees are trained and fully understand this policy. Permittees working and/or supporting the hosting of a special event on the ROW shall possess a TATC or designate a qualified individual with a TATC within each work group who will be responsible for the safety of the work group. The individual submitting and signing off on the application shall also take the Track Access Training. For any construction/maintenance work, all personnel who will be on-site shall have the training. For any special event, any personnel that will be setting up equipment of any kind within the Dynamic Safety Envelope shall also have the track access training.



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2. Definitions

2.1 Activity:

- Any construction, demolition, event, moving event, special event, excavation, use of right of way, cleaning, painting, repairing, or installation of traffic control devices, regardless of whether any of the previous requires approval of the City under the El Paso City Code; or
- Any activity under the El Paso City Code or other regulatory code that requires a permit, license, permission, or review from the City or other regulatory entity.

2.2 **Certified Personnel:** Personnel that have successfully completed the track access training. Certified personnel must participate in the Track Access Training bi-annually.

2.3 **Chief Safety Officer:** The person designated by the City Manager to oversee the safety of all Streetcar Infrastructure and Streetcar operations.

2.4 **City:** Means the City of El Paso, a home-rule municipality under Texas-Law.

2.5 **Construction:** Any non-special event work activity or activities performed by Permittee, utility, city department, and government entity, individual or on its behalf, with respect to its equipment, facilities, utility systems or other related materials, including any of the following activities performed within the operational right-of-way, under any streetcar infrastructure, or within 10 feet of any streetcar infrastructure:

- Installation, excavation, boring, laying, placement, upgrade, removal, extension, or replacement of utilities, facilities or other improvements, whether temporary or permanent;
- Modification or alteration to any surface, subsurface or aerial space;
- Performance, restoration or repair of pavement cuts or excavations.



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- 2.6 **Contact Wire (OCS Wire):** The wire in which the pantograph makes contact for electrical current collection. Normally made of copper or bronze, the wire is a single wire conductor usually with two grooves to which hangers and clamps may be fitted.
- 2.7 **Dispatcher/Dispatch:** A 24/7 point of contact for Sun Metro & Streetcar Operations, (915) 212-3454, or (915) 212-3425 [After Hours].
- 2.8 **Dynamic Safety Envelope:** See Operational Right-of-Way.
- 2.9 **Emergency:** In general, a set of circumstances reasonably requiring immediate access, construction or operations by Permittee to:
- Prevent imminent damage or injury to the life, health, or safety of any person (Tier 1 Emergency);
 - Restore or prevent material loss of utility service (Tier 2 Emergency); or
 - Prevent material damage to public or private property (Tier 3 Emergency).
- 2.10 **EPSC:** El Paso Streetcar.
- 2.11 **Event:** See Special Event and Activity.
- 2.12 **Fouling:** Any encroachment of workers, equipment, or materials within the defined EPSC Operational ROW.
- 2.13 **Guideway:** The concrete area immediately beneath, along, under, and surrounding the streetcar rails.
- 2.14 **Maintenance:** Any simple, small-scale activity or activities associated with regular and general upkeep of a building, equipment, machine, plant, landscape, or system against normal wear and tear. In addition, are performed by Permittee, utility, city department, and government entity, individual or on its behalf, with respect to its equipment, facilities, utility systems or other related materials, within the El Paso Streetcar (EPSC) Operational Right-of-Way (ROW), under any streetcar infrastructure or within 10 feet of any streetcar infrastructure.



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- 2.15 **MSF:** Maintenance & Storage Facility located at 601A Santa Fe St., El Paso, TX.
- 2.16 **MTD:** Mass Transit Department for the City of El Paso, a municipal transit department created under the Texas Transportation Code.
- 2.17 **Non-revenue Service Hours:** Shall mean all hours except for revenue service hours and the one-hour prior to the first revenue service hour of the day, or period of time when the streetcar service is scheduled to be inactive and revenue is not generated.
- 2.18 **Operational Right-of-Way:** The horizontal and vertical boundaries of the Streetcar track system, encompassing the overhead power contact wire and all associated infrastructure. This envelope includes the areas as shown in Exhibit C.
- 2.19 **Overhead Contact System (OCS):** All portions of the traction electrical system comprising of the overhead conductors (or single contact wire), aerial feeders, direct suspension system, supports, foundations, balance weights and other equipment or assemblies, that deliver electrical power to streetcar vehicles.
- 2.20 **Permittee:** The person, ROW-user, or entity applying for or receiving an authorization through COEP permitting system to perform construction, work, maintenance or special event within the EPSC right-of-way including:
- Any officer, director, partner, manager, superintendent, contractor, subcontractor or other authorized person-exercising control over or on behalf of the Permittee.
 - Notwithstanding anything to the contrary, the term Permittee includes the Permittee's officers, employees, contractors, subcontractors, and/or agents.
- 2.21 **Pole (OCS):** An independent vertical structure with or without guide wires, used to support the Overhead Contact System.
- 2.22 **Power Down:** The disconnection of electrical power to the entire Streetcar system (or any segment thereof) and/or the Maintenance & Storage Facility.
- 2.23 **Regulations:** All applicable federal, state or local statutes, laws, ordinances, codes, rules, and regulations.



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- 2.24 **Revenue Service Hours:** The period of time when the streetcar service is scheduled to be actively conveying passengers.
- 2.25 **Routine Maintenance (3rd Party):** Maintenance work performed by a company, franchisee, utility or City department on their infrastructure on a regular basis.
- 2.26 **ROW:** Right-of-way
- 2.27 **Sidewalk(s):** Means that portion of the public right-of-way, which is designated for the use, and movement of pedestrians.
- 2.28 **Special Event:** Non-construction activities performed in or on the El Paso Streetcar (EPSC) Operational Right-of-Way (ROW), under any streetcar infrastructure or within 10 feet of any streetcar infrastructure that has the potential to disrupt the normal operations of the EPSC.
- 2.29 **Street:** Means that portion of the public right-of-way improved, designed or ordinarily used for vehicular traffic.
- 2.30 **Standard Operating Procedures/Policy (SOP):** A written document of fixed instructions to handle specific situations that promote operational efficiency and effectiveness, safeguard assets and records, overcome safety issues, provide quality control, declare or encourage adherence to policies, and assure compliance with laws and regulations.
- 2.31 **Streetcar:** The rail mass transit system owned and operated by the City of El Paso.
- 2.32 **Streetcar Infrastructure:** Any component or item used for the operation of the Streetcar, including but not limited to, the railcars, track works, traction power substations, overhead contact systems, overhead contact system poles, train wayside communications systems, and/or the streetcar maintenance facility.
- 2.33 **Streetcar Vehicle:** An electrically driven rail transit vehicle designed for local public transportation that runs on rails in mixed traffic at surface road grade, which is powered by an overhead contact wire.
- 2.34 **Streetcar Flagger:** A trained person provided by the Permittee and who is designated to provide a warning to clear the tracks for personnel working within 10 feet of the tracks and the employee designated to communicate with an



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approaching Streetcar and other on-track equipment operators utilizing hand signals.

- 2.35 **Streetcar Safety Manager:** Individual responsible for the overall safety, security, and training of the system. May also serve as proxy in the absence of the Chief Safety Officer.
- 2.36 **Streetcar System:** Any rail, track, appurtenance, alignment guideway or facility located within a public right-of-way that is authorized by the City to be used for streetcars.
- 2.37 **Streetcar Stop:** A designated stop, shelter, location or facility used as a loading point for passengers.
- 2.38 **Sun Metro:** Sun Metro is the department responsible for operating and maintaining the El Paso Streetcar.
- 2.39 **Track Access Permit:** Official document, which allows any activity to take place on the streetcar right-of-way.
- 2.40 **Track Access Training:** Training provided by EPSC for all its employees, emergency personnel or first responders, public service providers, contractors, utilities or Permittees, and any others who will be working or performing a special event within the operational right-of-way. Emergency personnel or first responders will require additional training pertaining to field operations. Refresher training must be taken bi-annually.
- 2.41 **Track Access Training Card (TATC):** Card issued by Sun Metro to certify personnel that have completed Track Access Training. TATC must be on the person at all times while within the Operational Right-of-Way. Refer to Appendix L for a sample of El Paso Streetcar Track Access Card.
- 2.42 **Track Allocation:** The specific time allotted to permittee by EPSC to perform work or special event.
- 2.43 **Track Access Request Form:** Form required to request access to the EPSC system.

- For Construction, Maintenance, or Other purposes – Use Non-Special



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Event Track Access Request Form, Exhibit A

- For Special Event purposes – Use Special Event Track Access Request Form, Exhibit A1

2.44 **Traction Power Substation (TPSS):** A substation used to convert high voltage to low voltage in order to supply electrical power to the OCS and other current collection systems.

2.45 **Work:** Non-special event activities performed within 10 feet of the operational right-of-way, under any streetcar infrastructure, or within 10 feet of any streetcar infrastructure that has the potential to disrupt the normal operations of the EPSC.

3. Scope

3.1 This policy governs all Streetcar employees, contractors, and Permittees.

3.2 Ordinance 018824, section 13.42.020 (A) of the El Paso Municipal Code establishes permitting procedures for any activity occurring within the vicinity of the El Paso Streetcar Infrastructure. See Appendix M for ordinance.

3.3 The Chief Safety Officer has authority to create and revise the Track Access Program Policy for safeguarding the public health and safety in accordance with best practices, as well as, the protection of the Streetcar Infrastructure.

4. Procedures

4.1 Prior to any activity within the EPSC Operational ROW, the Permittee must submit the appropriate permits to the responsible department at the COEP, or any other regulatory entity as required. Planning/One Stop Shop/SM

4.2 The responsible department will send the permit request to El Paso Streetcar for evaluation IF the project is within the EPSC operational ROW. Streetcar personnel will evaluate the application and submit the appropriate comments and requirements back to the respective department.

4.3 A Track Access Request Form (Refer to Exhibit A or A1) and all required



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documentation must be submitted to EPSC. The following deadlines shall apply:

- Construction or other non-special event activity – **14 calendar days** minimum prior to the start of work.
- Special Events – **30 calendar days** minimum prior to the event.
- Trenching and/or excavation activity – **45 calendar days** minimum prior to the start of work.
- All applications will be submitted via the City of El Paso Online Permitting System using the following link.

<https://epermits.elpasotexas.gov/CitizenAccess/Default.aspx>

- 4.4 A request may be submitted by email, or in person. However, it is **REQUIRED** that the applicant participate in the Track Access Training prior to the submission of an access request. Track Access Training is available online through the Track Access Program Policy website: <http://www.sunmetro.net/streetcar/track-access>. For information regarding the online training/test, please reach out to the number listed below.

El Paso Streetcar
915-212-3465

- 4.6 It is the responsibility of the Permittee requesting **TO CONDUCT A SPECIAL EVENT, WORK, OR ACTIVITY ON, UNDER, ACROSS, OR OVER** the EPSC Operational ROW and the related infrastructure, as defined within this document, to coordinate with EPSC prior to any work or event.
- 4.7 Prior to being granted authority to work or conduct a special event within the EPSC Operational ROW, the Permittee is required to:
- Attend and complete Track Access Training.
 - Provide a completed "Track Access Form" (Exhibit A or A1) in accordance with the deadlines stated above.
 - Provide a work/site plan (if applicable), or drawings as necessary,



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illustrating proposed work/site locations and work or special event details. Work/site plan and drawings will describe the proposed work or special event, equipment, crew size, number of participants, vehicles to be used, height of vehicles (measured from pavement to highest vertical point on vehicle), and location of work or event. For construction related projects, construction drawings must be included.

- Provide a Traffic Control Plan and/or Pedestrian Control Plan in accordance with the Streetcar Specific Typical Applications as depicted in Exhibit D.
- Submit one (1) copy each of Commercial General Liability Insurance, with Contractual Liability – Railroads Endorsement **OR** Railroad Protective Liability Insurance with original endorsements. Refer to Exhibit E.
- The applicant may be required to meet with the Chief Safety Officer or designee in the field to clarify the work/site plan and determine any additional requirements.

FAILURE TO COMPLY WITH THE 14/30/45 – DAY REQUIREMENT WILL RESULT IN AN AUTOMATIC DENIAL OF THE TRACK ACCESS REQUEST APPLICATION.

4.8 When all requirements above have been submitted, EPSC will deem the application complete and begin review of the Track Access Request, only complete applications will be accepted. Please review the submission completeness checklist located in Exhibit A or A1 for further information. Once the submission has been reviewed, EPSC will provide the permittee with review comments for correction, if necessary.

ALL REVIEW COMMENTS MUST BE SATISFACTORILY ADDRESSED WITHIN 5 CALENDAR DAYS FOR SPECIAL EVENT OR CONSTRUCTION ACTIVITIES OR 15 CALENDAR DAYS FOR TRENCHING/EXCAVATION ACTIVITIES. FAILURE TO PROVIDE RESPONSE TO STAFF COMMENTS/CORRECTIONS WILL RESULT IN AN AUTOMATIC DENIAL OF THE TRACK ACCESS REQUEST.



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4.9 Once all review comments have been satisfactorily addressed, the applicant will receive an approved Track Allocation and an approved Track Access Permit. This permit, and all approved attachments must be available for inspection at all times during the event or work period.

4.10 The Track Access Request Form **DOES NOT REPLACE THE NEED TO ACQUIRE ANY PERMIT REQUIRED BY THE COEP OR OTHER REGULATORY ENTITY**, to include a Temporary Traffic Control permit issued by COEP Traffic Engineering. The Permittee must contact COEP Traffic Engineering or Texas Department of Transportation (TxDOT) for a Temporary Traffic Control Permit for work or event that may affect traffic or sidewalks as required by COEP or TxDOT.

4.11 If required by EPSC Operations, the Permittee is responsible for providing additional streetcar specific temporary traffic control in accordance with Exhibit D, and the most recent edition of the Texas Manual on Uniform Traffic Control Devices (TMUTCD). **EPSC DOES NOT PROVIDE** temporary traffic control.

4.12 The Permittee is responsible for satisfying all safety requirements including, but not limited to field supervision, communication device(s), certifications, reflective safety vests, hard hats, safety glasses, security, etc. In addition, Permittee shall submit proof of a safety meeting held prior to the commencement of work or special event, i.e. a sign-in sheet with safety topic discussed, names and signatures of all attendees. Submittals of a safety meeting sign-in sheet are as follows:

- **1 day of work/special event = 1 sign-in sheet**
- **1 week or more of work/special event = minimum of 1 sign-in sheet per week**
- **EMERGENCY WORK = 2 days or less, no sign-in sheet required, however, if the work extends past 2 days, need to follow schedule above.**

Please refer to Exhibit K for a sign-in sheet template or provide your own template with names and signatures of all who attended the meeting.

4.13 Permittee will stop work or event in progress and immediately notify EPSC if work/event deviates in any way from the written plans submitted and approved



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by EPSC.

4.14 The work or event may be stopped immediately by EPSC management, **with or without notice, at any time**. Typical conditions under which this could occur include, but are not limited to:

- Safety violations or issues.
- Scheduling violations or issues.
- Deviations from approved Track Access Request.
- Temporary traffic control not available.
- Any time EPSC management deems necessary.
- Any time COEP departments deem necessary.

4.15 Work or event performed by Permittee during operational hours shall not cause interference with the constant, continuous, and uninterrupted use of the Streetcar system or with other Permittees performing work or event along the EPSC Operational ROW.

4.16 When there exists multiple permits, or requests for access to the ROW it shall be the policy of EPSC to accept/process requests on a first come, first serve basis. Only one (1) Permittee may occupy the same requested portion of the ROW at a time, subsequent requests for the use of an already allocated portion of the ROW will be denied regardless of permission obtained from other COEP departments, contractors, vendors or other regulatory entity.

4.17 Under no circumstances, will a Permittee access tracks with vehicles, equipment, or machinery without permission of the EPSC Management.

4.18 Excavations within twenty-five (25) feet of the track centerline must comply with the Excavation Support Guidelines as presented in Exhibit H1.

4.19 Cables crossing the ROW must comply with the most current National Electric Safety Code (NESC) clearances and pertinent EPSC requirements as



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presented in Exhibit I.

- 4.20 Permittee must contact El Paso Streetcar Dispatch at **(915) 212-3454, or (915) 212-3425 [After Hours]** thirty minutes prior to entering the ROW, and when exiting the ROW.
- 4.21 Permittee will not foul tracks without prior authorization. Flagger(s) are only required during revenue-service hours.
- 4.22 In the event that the Permittee is authorized by EPSC to perform work on the EPSC Operational ROW that moves, disturbs, or modifies EPSC property in any manner, the Permittee shall, at its own expense, restore the property to the same condition before the work was performed. Such restoration must be to the satisfaction of the EPSC Superintendent of Operations and/or the EPSC Safety Manager and either of their designees, as well as all applicable COEP departmental requirements and design standards.
- 4.23 The Permittee must comply with all applicable federal, state, local laws, regulations, and standards affecting its work or event.
- 4.24 As a limitation to any rights or licenses that may be granted to the Permittee, EPSC reserves the right to use and maintain its entire property. This includes EPSC's right to construct, maintain, repair, renew, use, operate, change, or modify: platforms, signal, communication, fiber optics, power, or other wire lines, pipelines, and other facilities upon, along, or across any or all parts of its property. All or any of the above-mentioned use and maintenance may be freely done at any time or times by EPSC without liability to the Permittee or any other party for compensation or damages.
- 4.25 In accordance with EPSC's "Track Access Program" requirements, and in exchange for the authorization to perform work or event on or near the EPSC Operational ROW, **PERMITTEE AGREES, TO THE FULLEST EXTENT ALLOWED BY LAW, TO INDEMNIFY, DEFEND, AND HOLD HARMLESS THE CITY, MTD AND THE CITY'S AND MTD'S OFFICERS, AGENTS, AND EMPLOYEES FROM AND AGAINST ANY AND ALL CLAIMS, SUITS, ACTIONS JUDGMENTS, FINES, PENALTIES, LOSS, DAMAGE, COSTS, OR EXPENSE (INCLUDING BUT NOT LIMITED TO ATTORNEYS' FEES), WHETHER DIRECT OR INDIRECT, DUE TO BODILY OR PERSONAL INJURY, DEATH, SICKNESS, PROPERTY DAMAGE, OR PROPERTY LOSS**



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ARISING OUT OF PERMITTEE'S ACTIONS, OMMISIONS, AND/OR ACTIVITIES. IN THE EVENT A THIRD PARTY MAKES A CLAIM OR FILES A LAWSUIT AGAINST THE CITY AND/OR MTD FOR ANYTHING RELATED TO PERMITTEE'S ACTIONS, OMMISIONS, OR ACTIVITIES, THE PERMITTEE SHALL DEFEND SUCH CLAIM OR LAWSUITS ON BEHALF OF THE CITY AND/OR MTD AT PERMITTEE'S SOLE COST AND EXPENSE. PERMITTEE FURTHER AGREES TO REPAIR ANY DAMAGE OR DISTURBANCE TO CITY AND/OR MTD PROPERTY CAUSED BY PERMITTEE.

5. Emergency Access

- 5.1 Emergency Access will be granted on a case-by-case basis in accordance with the provisions outlined in this policy. The Chief Safety Officer shall have the sole responsibility in granting emergency access.
- 5.2 Request for emergency access shall be routed to Streetcar Dispatch at **915-212-3454 or 915-212-3425** [After Hours].
- 5.3 Submittal requirements (see section 2.9 for definitions):
 - Tier 1 – Documented written or verbal description of the emergency;
 - Tier 2 or 3 – Track Access Request Form **AND** Traffic Control Plan.
- 5.4 All work for Tier 2 or Tier 3 activities shall only be performed during non-revenue service hours.
- 5.5 Emergency Access will be granted for a period of **5 calendar days maximum**, after which time the permittee shall submit a Track Access Request and provide all required documentation in accordance with the provisions outlined in this policy.

6. Responsibilities

- 6.1 It is the responsibility of all Streetcar employees, and Permittees to know, understand and abide by this policy.

7. References

- 7.1 APTA RT-OP-S-020-14, Rail Transit Track Allocation Program Requirements – March 31, 2014.



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7.2 City of El Paso Municipal Code, Title 13, Chapter 13.42 – Streetcar Infrastructure.

7.3 Texas Manual on Uniform Traffic Control Devices – Revision 2, October 2014.

8. Summary of Changes

8.1 – On file.



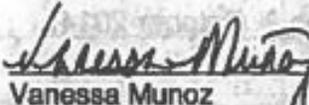
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Prepared By:


Vanessa Munoz
Senior Service Planner
Streetcar ROW Compliance

6/15/2021
Date:

Reviewed By:

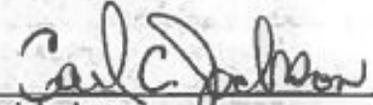

Everett Esparza
Superintendent
Streetcar Operations

6/16/2021
Date:

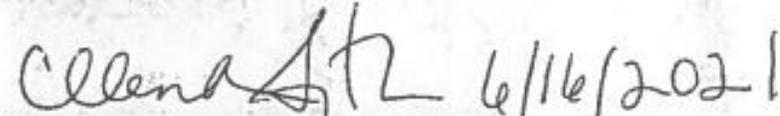

Johnny Balcazar
Manager
Streetcar Safety

6/16/2021
Date:

Approved By:


Carl Jackson
Assistant Director
Streetcar Operations

6/16/2021
Date:


Ellen A. Smyth
Accountable Executive

6/16/2021
Date:



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List of Exhibits

1. Exhibit A – Track Access Request Form – Non-Special Event
2. Exhibit A1 – Track Access Request Form – Special Event
3. Exhibit B – El Paso Streetcar System Map
4. Exhibit C – Operational Right-of-Way / Dynamic Safety Envelope
5. Exhibit D – Temporary Traffic Control
6. Exhibit E – Insurance Requirements
7. Exhibit F – Permit Requirements
8. Exhibit G – Crane Staging and Placement Standards
9. Exhibit H – Trenching Standards
10. Exhibit H1 – Excavation Support Guidelines H1 is figure. F1 p.
11. Exhibit I – Vertical Clearance Standards
12. Exhibit J – Directional Boring Standards
13. Exhibit K – Safety Meeting Sign-in Sheet Template
14. Exhibit L – Track Access Training Card Template
15. Exhibit M – Ordinance 018824, section 13.42.020 (A)

Exhibit A and A1

Track Access Request Form(s)



Submission Completeness Checklist: Non-Special Events

REQUIRED DOCUMENTATION FOR STREETCAR TRACK ACCESS PERMIT

- SUBMISSION DEADLINES** – The following deadlines shall apply:
- Construction, Maintenance, or other Non-Special Event activity – **14 calendar days** minimum prior to the start of work.
 - Trenching and/or Excavation activity – **45 calendar days** minimum prior to the start of work.

FAILURE TO COMPLY WITH THE 14/45 – DAY REQUIREMENT WILL RESULT IN AN AUTOMATIC DENIAL OF THE TRACK ACCESS REQUEST APPLICATION.

- TRACK ACCESS TRAINING** – A track access training shall be completed **PRIOR TO** submittal of the Track Access Permit application. A valid ID# shall be required on the application at the time of submission. Track Access Training is available online through the Track Access Program Policy website: <http://www.sunmetro.net/streetcar/track-access>. For information regarding the online training/test, please reach out to 915-212-3465.

- APPLICATION** – Each item on this application shall be completed and all documentation required on this form shall be submitted before this application is accepted for processing, to include all signatures/initials. Submittal of an application does not constitute acceptance for processing until Streetcar staff reviews the application for accuracy and completeness.

- CONSTRUCTION DRAWINGS** – Submit construction drawings of proposed scope of work, if applicable. For maintenance activities, provide detailed description of work in application section 3.0. Refer to the following exhibits for specific submittal requirements:

- Exhibit H – Trenching and/or Excavations
- Exhibit I – Aerial Applications (fiber optic, communications, power, etc.)
- Exhibit J – Directional Boring

- RAIL INSURANCE**- All required insurance shall be submitted with the application as stipulated in Exhibit E of the Track Access Program Policy.
- TRAFFIC CONTROL PLAN** – A traffic control plan shall be submitted with the application as stipulated in Exhibit D of the Track Access Program Policy.
- SAFETY MEETING SIGN-IN SHEET** – A sign-in sheet shall be submitted after the issuance of a track access permit and prior to any work/special event taking place on the streetcar right-of-way. Refer to Exhibit K of the Track Access Program Policy for a sign-in sheet template or provide your own template with printed name and signature of all that attended.



4.0 **Limit of Access** (describe location of work area, example – Santa Fe from Franklin to Paisano or the intersection of Oregon and University):

5.0 **Do you have a valid Track Access Training ID# ?** Yes, provide ID #: _____

****A Valid Track Access Training ID# is required at time of application submission, failure to provide a valid ID# will result in an automatic denial of application****

Track Access Training is provided online through the Track Access Program Policy website: <http://www.sunmetro.net/streetcar/track-access>. For information regarding the online training/test, please contact Moraima Cervantes at 915-212-3465.

6.0 **Does your work include the use of a crane, forklift, lifting device, boom truck, scissor lift or similar vehicle?** Yes No

6.1 *If Yes, provide a description of vehicle(s):*

6.2 Vehicle Height*: _____ Vehicle Width: _____ Number of Vehicles: _____ Maximum Reach or Extent of Boom, Crane, Lift: _____
* Vehicle height measured from ground level to top most element of vehicle.

7.0 **Does your work include the use of a stage, tent, canopy, elevated platform, scaffolding or any temporary structure?** Yes No

7.1 *If Yes, provide a description of structure:*

7.2 Structure Height*: _____ Structure Length: _____ Structure Depth: _____
* Structure height measured from ground level to top most element.

Acknowledgements

8.0 I _____ **(the "Applicant")** understand that any encroachment into the Streetcar Operational Right-of-Way will require Streetcar Specific Temporary Traffic Control and the use of a Streetcar Flagger (if during revenue-service hours), as referenced in Exhibit D. I also understand that an approved Traffic Control Plan (TCP) or Pedestrian Control Plan (PCP) will also be required and that all flaggers and signage meet Texas Department of Transportation (TxDOT) standards incorporated in the most recent edition of the Texas Manual on Uniform Traffic Control Devices (TMUTCD). Furthermore, I understand that the City of El Paso will not furnish a Streetcar Flagger on my behalf, and that it is the sole responsibility of the applicant to contract with a traffic control provider. _____ **(initials)**

8.1 I understand when there exists multiple permits, or requests for access to the ROW it shall be the policy of EPSC to accept/process requests on a first come, first serve basis. Only one (1) Permittee may occupy the same requested portion of the ROW at a time, subsequent requests for the use of an already allocated portion of the ROW will be denied regardless of permission obtained from other COEP departments, contractors, vendors, or other regulatory entity. _____ **(initials)**

8.2 I understand that I must provide insurance as a condition of this permit application in accordance with Exhibit E. _____ **(initials)**

Applicant will coordinate with applicable City Departments, state, or federal agencies to obtain any necessary permits and approvals required for the proposed work and use of premises.

Applicant releases the City of El Paso ("City"), the Mass Transit Department for the City of El Paso ("MTD") and the City's and MTD's officers, employees, and agents from any liability and claims for illness, injuries, death, property damage, and/or property loss sustained by Applicant as a result of any inaction or action by the Applicant or third party on the public right-of-way.

APPLICANT AGREES, TO THE FULLEST EXTENT ALLOWED BY LAW, TO INDEMNIFY, DEFEND, AND HOLD HARMLESS THE CITY OF EL PASO (THE "CITY"), THE MASS TRANSIT DEPARTMENT FOR THE CITY OF EL PASO ("MTD") AND THE CITY'S AND MTD'S OFFICERS, AGENTS, AND EMPLOYEES FROM AND AGAINST ANY AND ALL CLAIMS, SUITS, ACTIONS JUDGEMENTS, FINES, PENALTIES, LOSS, DAMAGE, COST, OR EXPENSE (INCLUDING BUT NOT LIMITED TO ATTORNEYS' FEES), WHETHER DIRECT OR INDIRECT, DUE TO BODILY OR PERSONAL INJURY, DEATH, SICKNESS, PROPERTY DAMAGE, OR PROPERTY LOSS ARISING OUT OF APPLICANT'S ACTIONS OMMISIONS, AND/OR ACTIVITIES. IN THE EVENT A THIRD PARTY MAKES A CLAIM OR FILES A LAWSUIT AGAINST THE CITY AND/OR MTD FOR ANYTHING RELATED TO APPLICANT'S ACTIONS, OMMISIONS, OR ACTIVITIES, THE APPLICANT SHALL DEFEND SUCH CLAIM OR LAWSUITS ON BEHALF OF THE CITY AND/OR MTD AT APPLICANT'S SOLE COST AND EXPENSE. APPLICANT FURTHER AGREES TO REPAIR ANY DAMAGE OR DISTURBANCE TO CITY AND/OR MTD PROPERTY CAUSED BY THE APPLICANTS. APPLICANT AGREES THAT FOR PURPOSES OF THIS PROVISION THE ACTIONS, OMISSIONS, AND/OR ACTIVITIES INCLUDE THOSE OF THE APPLICANT'S AGENTS, EMPLOYEES, OFFICERS, CONTRACTORS, AND SUBCONTRACTORS.

ACKNOWLEDGEMENT: I attest that this application is complete and accurate to the best of my knowledge. I understand that any inaccurate or incomplete information provided on this application may delay or be cause for denial of the Track Access Permit. Furthermore, I attest that I have read, fully understand, and accept all conditions and requirements of the Track Access Program Policy.

Submit Completed Applications through the City of El Paso Online Permitting System:

<https://epermits.elpasotexas.gov/CitizenAccess/Default.aspx>

8.4 Applicant Name: _____

8.5 Applicant Signature: _____ Date: _____

Submission Completeness Checklist: Special Events

REQUIRED DOCUMENTATION FOR STREETCAR TRACK ACCESS PERMIT

- SUBMISSION DEADLINES** – The following deadlines shall apply:
- Special Event activity – **30 calendar days** minimum prior to the start of the event.

FAILURE TO COMPLY WITH THE 30 – DAY REQUIREMENT WILL RESULT IN AN AUTOMATIC DENIAL OF THE TRACK ACCESS REQUEST APPLICATION.

- TRACK ACCESS TRAINING** – A track access training shall be completed **PRIOR TO** submittal of the Track Access Permit application. A valid ID# shall be required on the application at the time of submission. Track Access Training is available online through the Track Access Program Policy website: <http://www.sunmetro.net/streetcar/track-access>. For information regarding the online training/test, please reach out to 915-212-3465.

- APPLICATION** – Each item on this application shall be completed and all documentation required on this form shall be submitted before this application is accepted for processing, to include all signatures/initials. Submittal of an application does not constitute acceptance for processing until Streetcar staff reviews the application for accuracy and completeness.

- SITE PLAN/MAP** – Submit a site plan or map detailing the event limits or route for any “moving” events.

- RAIL INSURANCE**- All required insurance shall be submitted with the application as stipulated in Exhibit E of the Track Access Program Policy.

- TRAFFIC CONTROL PLAN** – A traffic control plan shall be submitted with the application as stipulated in Exhibit D of the Track Access Program Policy.

- SAFETY MEETING SIGN-IN SHEET** – A sign-in sheet shall be submitted after the issuance of a track access permit and prior to any work/special event taking place on the streetcar right-of-way. Refer to Exhibit K of the Track Access Program Policy for a sign-in sheet template or provide your own template with printed name and signature of all that attended.



Exhibit A1 Track Access Request - Special Event



1.0 **Date of Submission:** _____

1.1 **Date of Event:** _____

1.2 **Hours of Event:** From _____ To _____

Accela CSSN

Office Use Only

DOWNTOWN UPTOWN

CSEV: _____

Office Use Only

2.0 **Requestor:** _____

2.1 **Title:** _____

2.2 **Company:** _____

2.3 **Address:** _____

2.4 **Cell Number:** _____

2.5 **Email Address:** _____

3.0 **Description of Event:**

4.0 **Number of Participants:** _____

5.0 **Limit of Access** (*describe location of event area, example – Santa Fe from Franklin to Paisano or the intersection of Oregon and University*):

6.0 **Do you have a valid Track Access Training ID# ?** Yes, provide ID #: _____

****A Valid Track Access Training ID# is required at time of application submission, failure to provide a valid ID# will result in an automatic denial of application****

*Track Access Training is provided online through the Track Access Program Policy website:
<http://www.sunmetro.net/streetcar/track-access>. For information regarding the online training/test,
 please contact Moraima Cervantes at 915-212-3465.*

7.0 Is your Special Event considered a “Moving” Special Event? (i.e., Walk, Run, Marathon, March, Parade)

Yes No

If yes, Streetcar Specific Temporary Traffic Control as referenced in Exhibit D, Typical Applications is required at time of application submission. Failure to provide a Traffic Control Plan will result in an automatic denial of application.

8.0 Does your event include the use of vehicles or parade floats? Yes No

8.1 *If Yes, provide a description of vehicle(s):*

8.2 Vehicle Height*: _____ Vehicle Width: _____ Number of Vehicles: _____
* Vehicle height measured from ground level to top most element of vehicle or float.

9.0 Does your event include the use of a stage, tent, canopy, elevated platform, or any temporary structure? Yes No

9.1 *If Yes, provide a description of structure:*

9.2 Structure Height*: _____ Structure Length: _____ Structure Depth: _____
* Structure height measured from ground level to top most element.

Acknowledgements

10.0 I _____ **(the “Applicant”)** understand that any encroachment into the Streetcar Operational Right-of-Way will require Streetcar Specific Temporary Traffic Control and the use of a Streetcar Flagger (if during revenue-service hours), as referenced in Exhibit D. I also understand that an approved Traffic Control Plan (TCP) or Pedestrian Control Plan (PCP) will also be required and that all flaggers and signage meet Texas Department of Transportation (TxDOT) standards incorporated in the most recent edition of the Texas Manual on Uniform Traffic Control Devices (TMUTCD). Furthermore, I understand that the City of El Paso will not furnish a Streetcar Flagger on my behalf, and that it is the sole responsibility of the applicant to contract with a traffic control provider. _____ **(initials)**

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ACKNOWLEDGEMENT: I attest that this application is complete and accurate to the best of my knowledge. I understand that any inaccurate or incomplete information provided on this application may delay or be cause for denial of the Track Access Permit. Furthermore, I attest that I have read, fully understand, and accept all conditions and requirements of the Track Access Program Policy.

10.4 Applicant Name: _____

10.5 Applicant Signature: _____ Date: _____

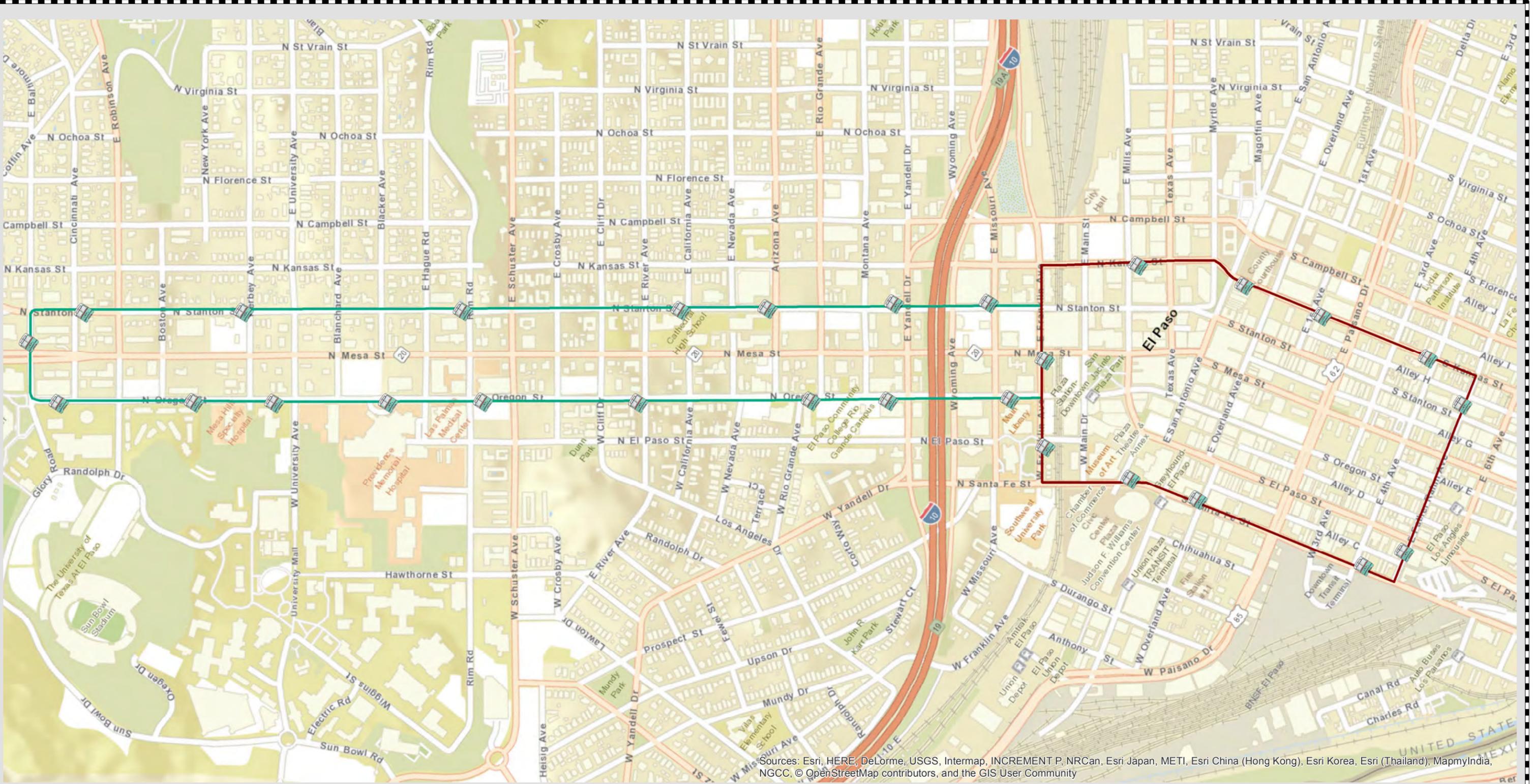
**Submit Completed Applications through the City of El Paso
Online Permitting System:**

<https://epermits.elpasotexas.gov/CitizenAccess/Default.aspx>

Exhibit B

El Paso Streetcar System Map





Sources: Esri, HERE, DeLorme, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), MapmyIndia, NGCC, © OpenStreetMap contributors, and the GIS User Community

El Paso Streetcar System Map - Exhibit B

-  Downtown Loop
-  Uptown Loop
-  Streetcar Stop



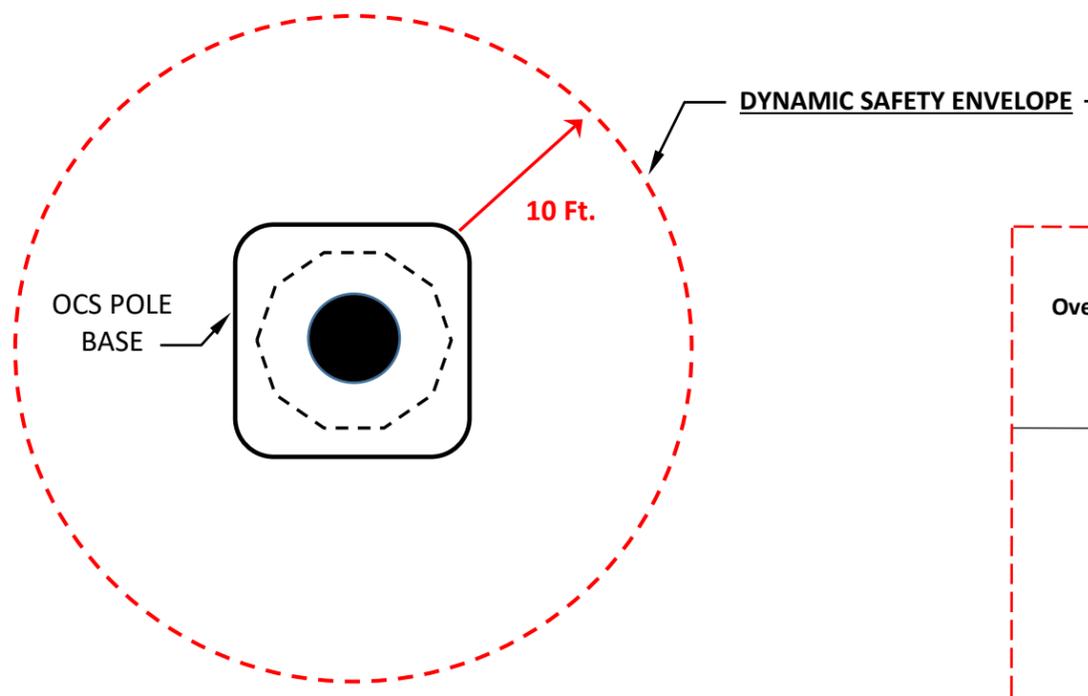
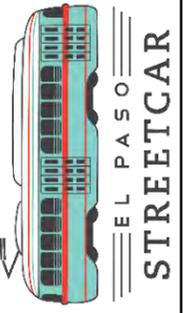
Cartography By: Michael J. Villa on 12/21/18
Data Source: City of El Paso/Sun Metro



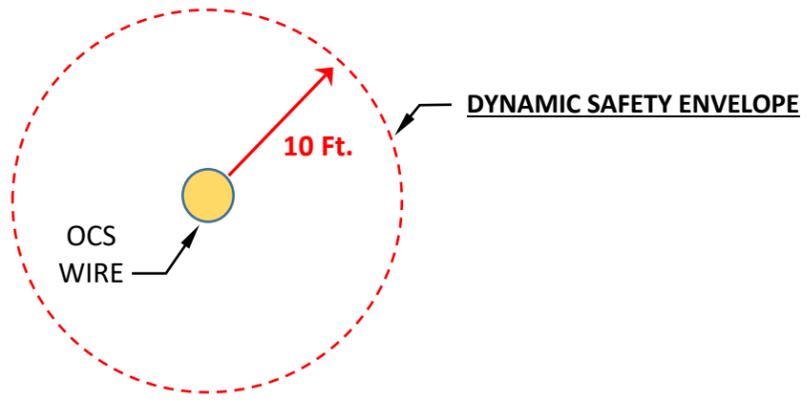
Exhibit C

Operational Right-of-Way / Dynamic Safety Envelope





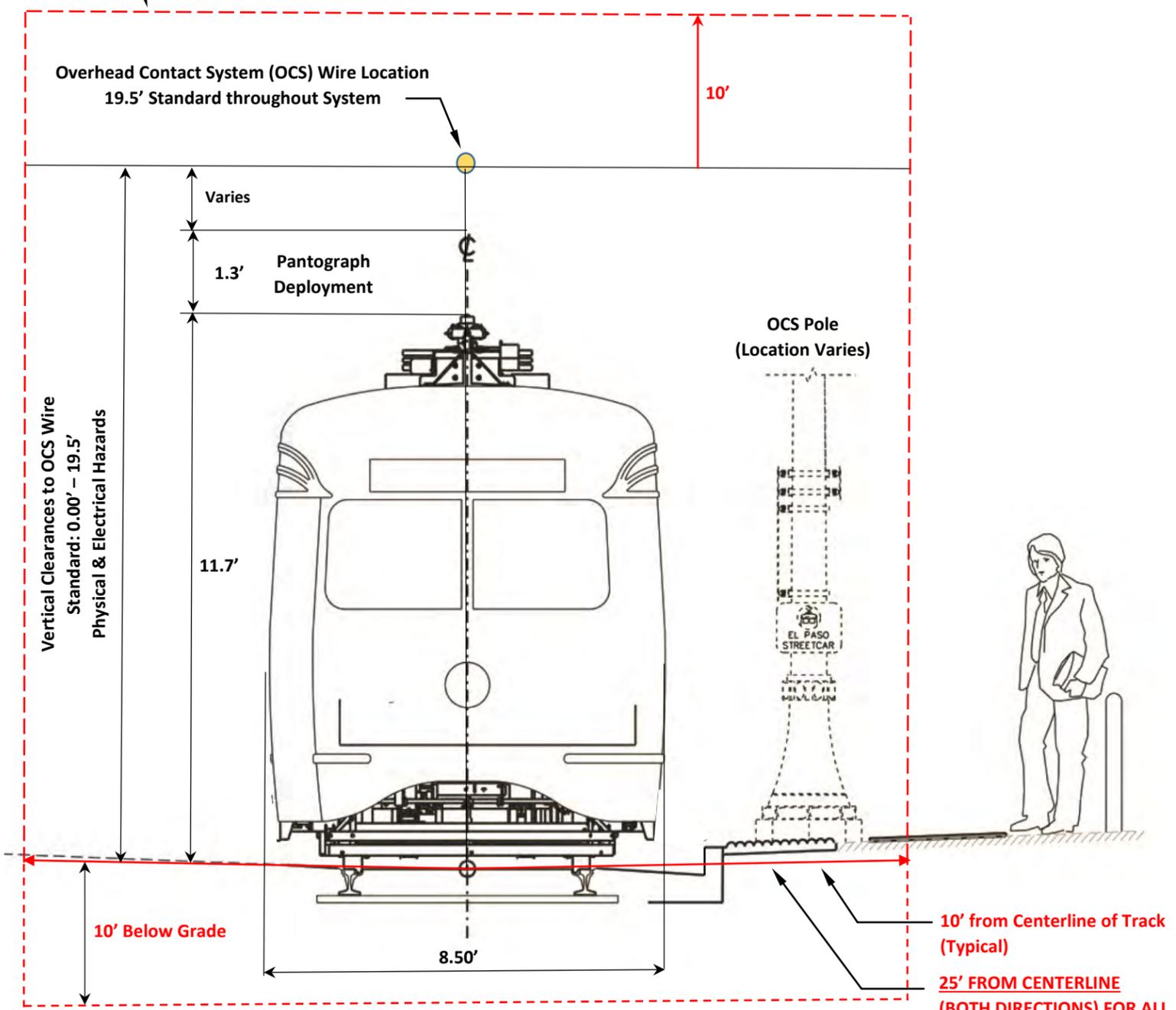
TYPICAL PLAN VIEW – OVERHEAD CONTACT SYSTEM (OCS) POLE – SCALE: N.T.S.



TYPICAL SECTION VIEW – OVERHEAD CONTACT SYSTEM (OCS) WIRE – SCALE: N.T.S.

GENERAL NOTES:

1. THE DYNAMIC SAFETY ENVELOPE (DSE) EXTENDS 10 FEET Laterally FROM THE CENTERLINE OF THE TRACK, 10 FEET SURROUNDING THE OCS WIRE, AND 10 FEET SURROUNDING ANY OTHER STREETCAR INFRASTRUCTURE, INCLUDING OCS POLES AND APPURTENANCES.
2. THE DSE ALSO EXTENDS 10 FEET BELOW GRADE FROM THE TOP OF THE FINISHED TRACK ELEVATION.
3. THE STREETCAR ZONE OF INFLUENCE EXTENDS 25 FEET FROM THE CENTERLINE OF THE TRACK FOR ALL EXCAVATIONS.
4. PHYSICAL AND ELECTRICAL HAZARDS EXIST ANYWHERE INSIDE THE DYNAMIC SAFETY ENVELOPE.
5. COORDINATION WITH EL PASO STREETCAR IS REQUIRED FOR ANY WORK INSIDE THE DSE.



TYPICAL SECTION VIEW – OPERATIONAL RIGHT-OF-WAY / DYNAMIC SAFETY ENVELOPE – SCALE: N.T.S.

Exhibit D

Temporary Traffic Control

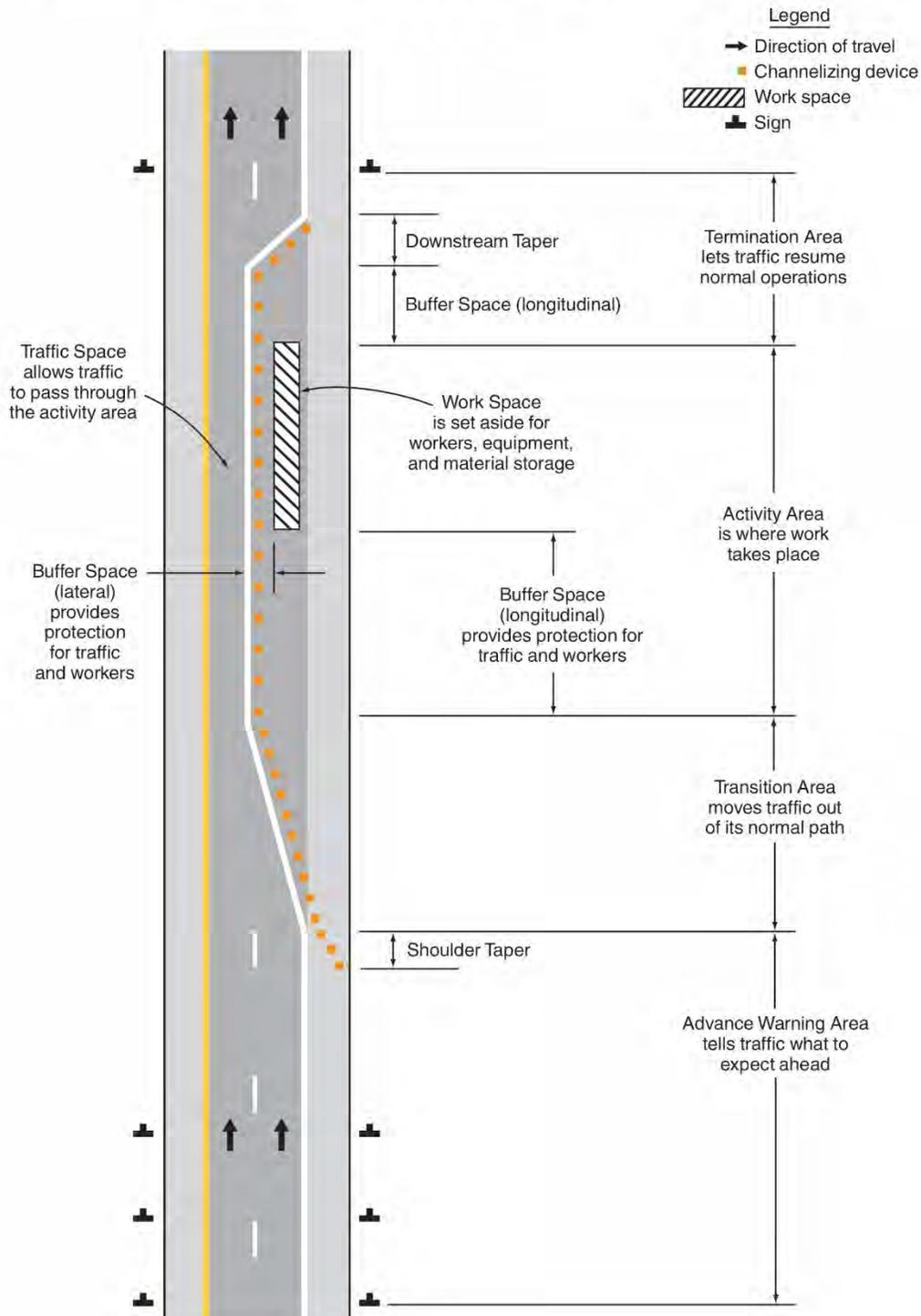


Streetcar Temporary Traffic Control Requirements

All Temporary Traffic Control (TTC) shall be in strict accordance with the most recent edition of the Texas Manual on Uniform Traffic Control Devices (TMUTCD) and the Streetcar Typical Application(s) referenced in this exhibit.

Components of Temporary Traffic Control

Figure 6C-1. Component Parts of a Temporary Traffic Control Zone



Requirements for Streetcar Flaggers

Section 6E.07 Flagger Procedures

Support:

01 The use of paddles and flags by flaggers is illustrated in Figure 6E-3.

Standard:

02 **Flaggers shall use a STOP/SLOW paddle, a flag, or an Automated Flagger Assistance Device (AFAD) to control road users approaching a TTC zone. The use of hand movements alone without a paddle, flag, or AFAD to control road users shall be prohibited except for law enforcement personnel or emergency responders at incident scenes as described in Section 6I.01.**

03 **The following methods of signaling with paddles shall be used:**

- A. **To stop road users, the flagger shall face road users and aim the STOP paddle face toward road users in a stationary position with the arm extended horizontally away from the body. The free arm shall be held with the palm of the hand above shoulder level toward approaching traffic.**
- B. **To direct stopped road users to proceed, the flagger shall face road users with the SLOW paddle face aimed toward road users in a stationary position with the arm extended horizontally away from the body. The flagger shall motion with the free hand for road users to proceed.**
- C. **To alert or slow traffic, the flagger shall face road users with the SLOW paddle face aimed toward road users in a stationary position with the arm extended horizontally away from the body.**

Option:

04 To further alert or slow traffic, the flagger holding the SLOW paddle face toward road users may motion up and down with the free hand, palm down.

Standard:

05 **The following methods of signaling with a flag shall be used:**

- A. **To stop road users, the flagger shall face road users and extend the flag staff horizontally across the road users' lane in a stationary position so that the full area of the flag is visibly hanging below the staff. The free arm shall be held with the palm of the hand above shoulder level toward approaching traffic.**
- B. **To direct stopped road users to proceed, the flagger shall face road users with the flag and arm lowered from the view of the road users, and shall motion with the free hand for road users to proceed. Flags shall not be used to signal road users to proceed.**
- C. **To alert or slow traffic, the flagger shall face road users and slowly wave the flag in a sweeping motion of the extended arm from shoulder level to straight down without raising the arm above a horizontal position. The flagger shall keep the free hand down.**

Guidance:

06 *The flagger should stand either on the shoulder adjacent to the road user being controlled or in the closed lane prior to stopping road users. A flagger should only stand in the lane being used by moving road users after road users have stopped. The flagger should be clearly visible to the first approaching road user at all times. The flagger also should be visible to other road users. The flagger should be stationed sufficiently in advance of the workers to warn them (for example, with audible warning devices such as horns or whistles) of approaching danger by out-of-control vehicles. The flagger should stand alone, away from other workers, work vehicles, or equipment.*

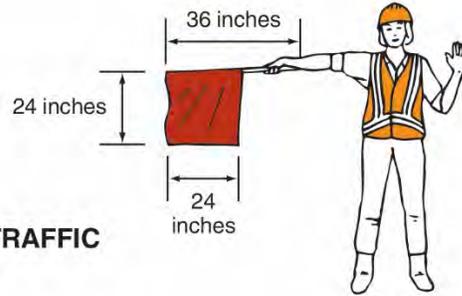
Option:

07 At spot lane closures where adequate sight distance is available for the reasonably safe handling of traffic, the use of one flagger may be sufficient.

Figure 6E-3. Use of Hand-Signaling Devices by Flaggers

**PREFERRED METHOD
STOP/SLOW Paddle**

**EMERGENCY SITUATIONS ONLY
Red Flag**



**TO LET
TRAFFIC PROCEED**



**TO ALERT AND
SLOW TRAFFIC**



Guidance:

- 08 *When a single flagger is used, the flagger should be stationed on the shoulder opposite the spot lane closure or work space, or in a position where good visibility and traffic control can be maintained at all times.*

Section 6E.08 Flagger Stations

Standard:

- 01 **Flagger stations shall be located such that approaching road users will have sufficient distance to stop at an intended stopping point.**

Option:

- 02 The distances shown in Table 6E-1, which provides information regarding the stopping sight distance as a function of speed, may be used for the location of a flagger station. These distances may be increased for downgrades and other conditions that affect stopping distance.

Guidance:

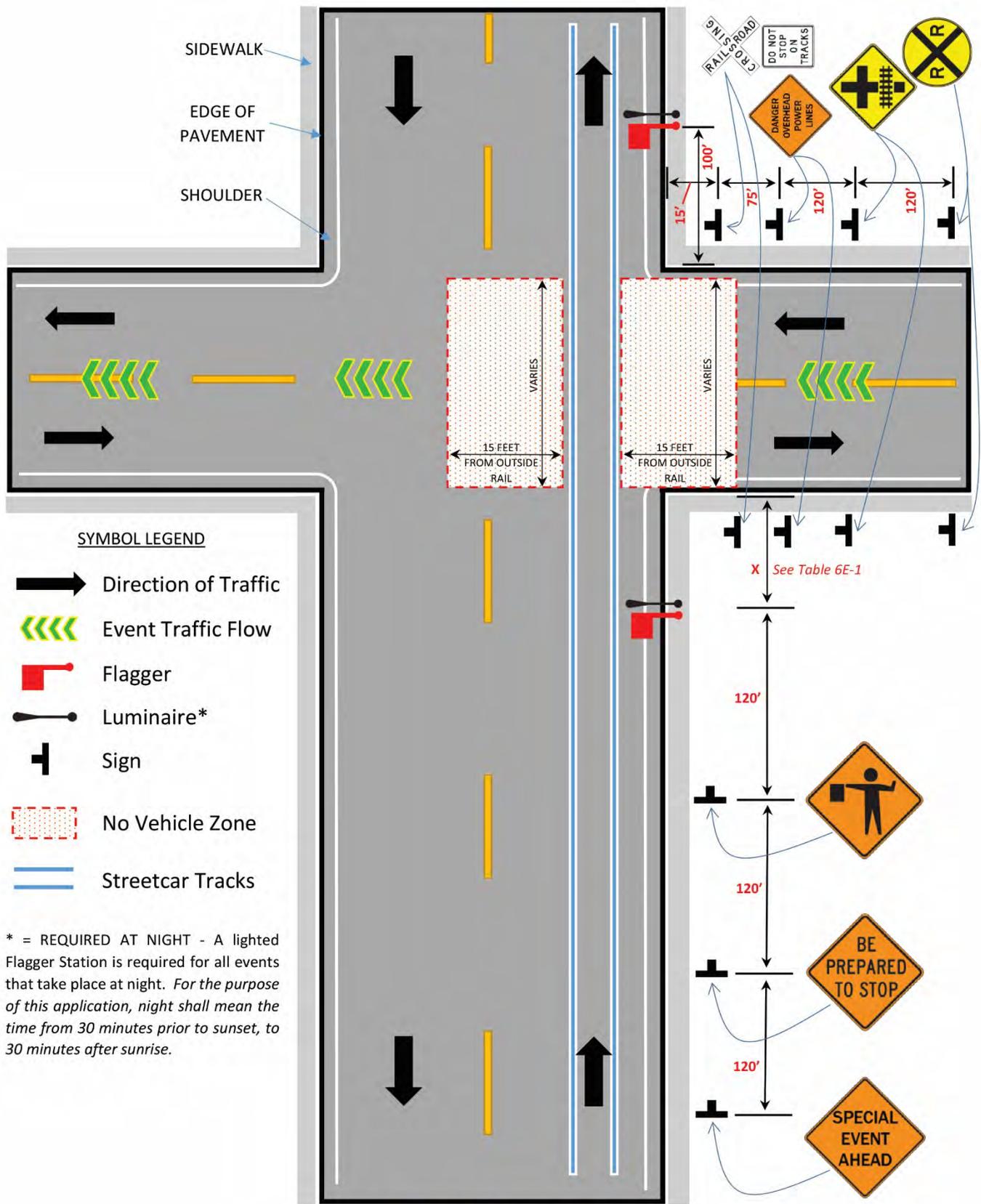
- 03 *Flagger stations should be located such that an errant vehicle has additional space to stop without entering the work space. The flagger should identify an escape route that can be used to avoid being struck by an errant vehicle.*

Standard:

- 04 **Except in emergency situations, flagger stations shall be preceded by an advance warning sign or signs. Except in emergency situations, flagger stations shall be illuminated at night.**

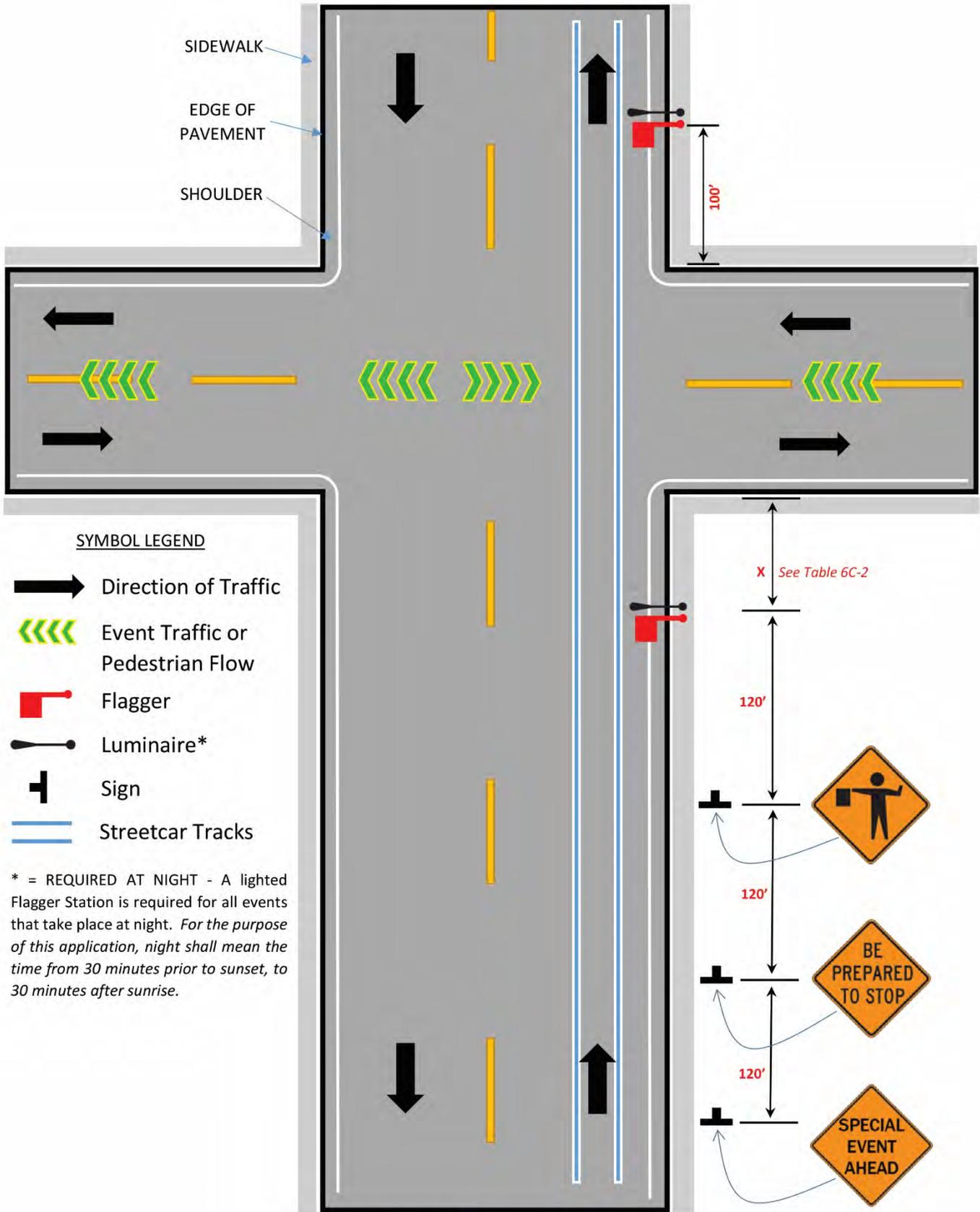
CONTINUED ON NEXT PAGE

Non-Static (Moving) Special Events within the Streetcar Operational ROW (Revenue Service Hours)



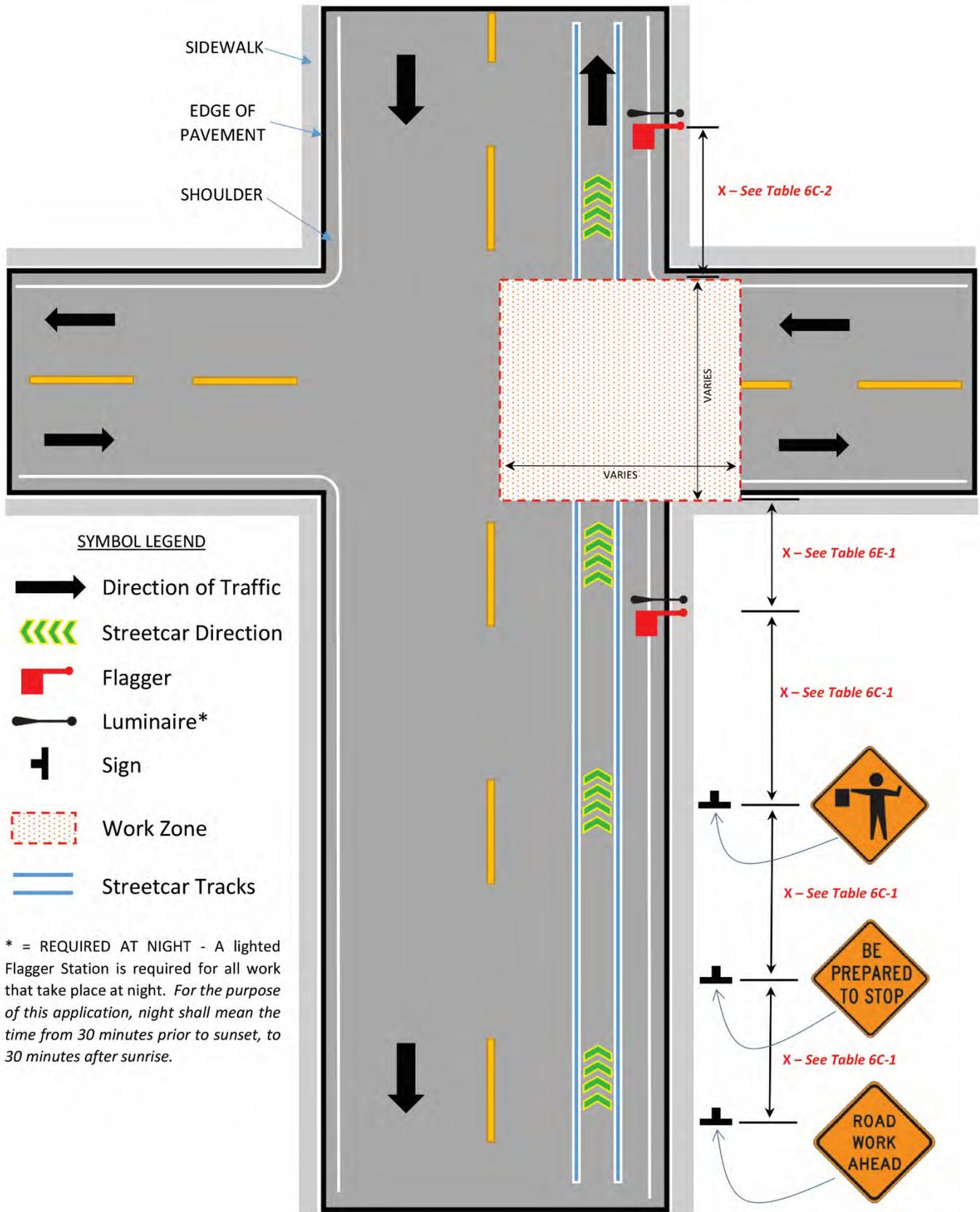
Typical Application 1

SPECIAL USE ONLY in CONJUNCTION with other Typical Application(s) as Required by Permit Official- (Revenue Service Hours)



Typical Application 1-B

Work Zones Within the Streetcar Operational ROW (Revenue Service Hours)



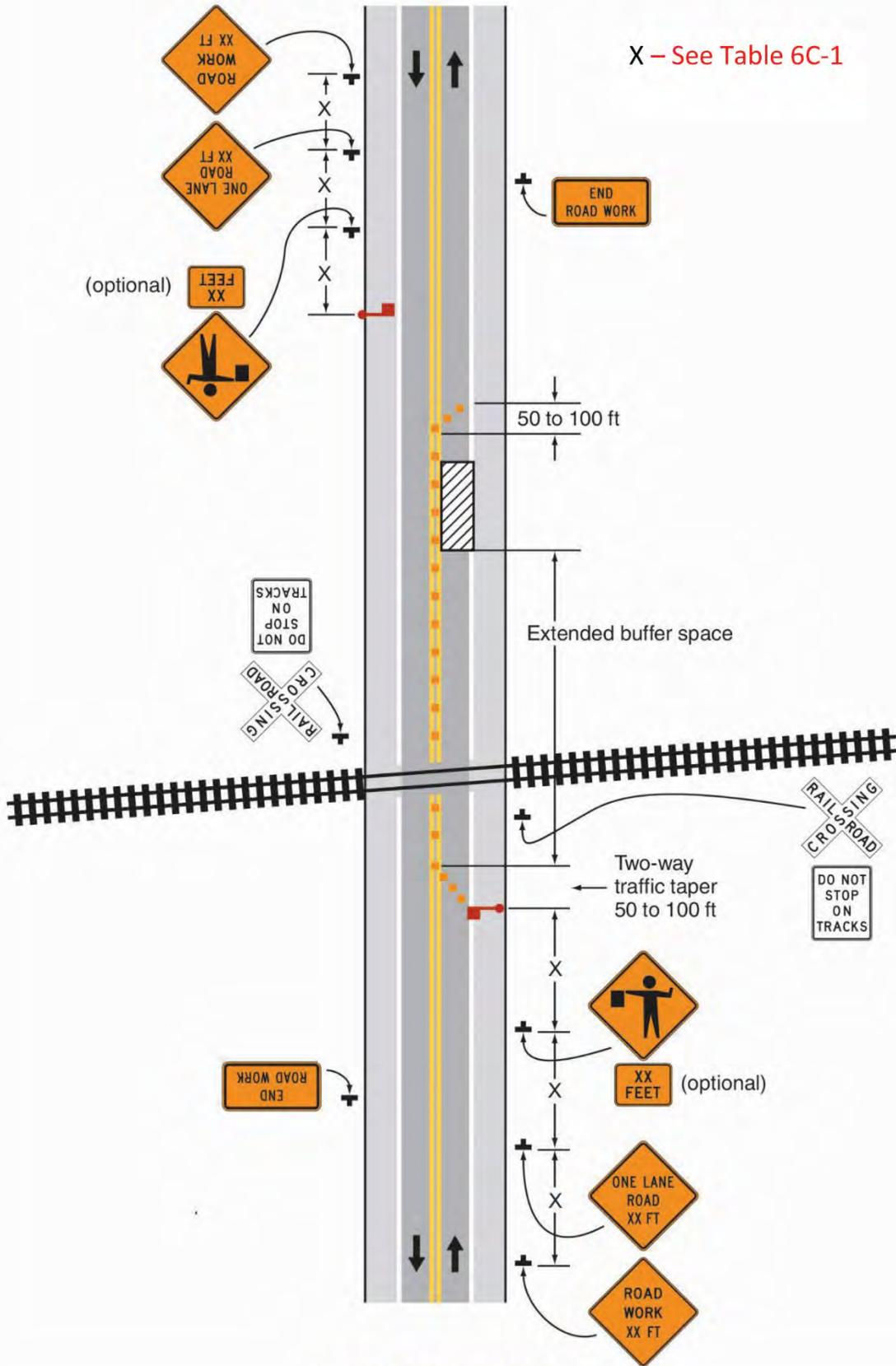
SYMBOL LEGEND

- Direction of Traffic
- Streetcar Direction
- Flagger
- Luminaire*
- Sign
- Work Zone
- Streetcar Tracks

* = REQUIRED AT NIGHT - A lighted Flagger Station is required for all work that take place at night. For the purpose of this application, night shall mean the time from 30 minutes prior to sunset, to 30 minutes after sunrise.

Typical Application 2

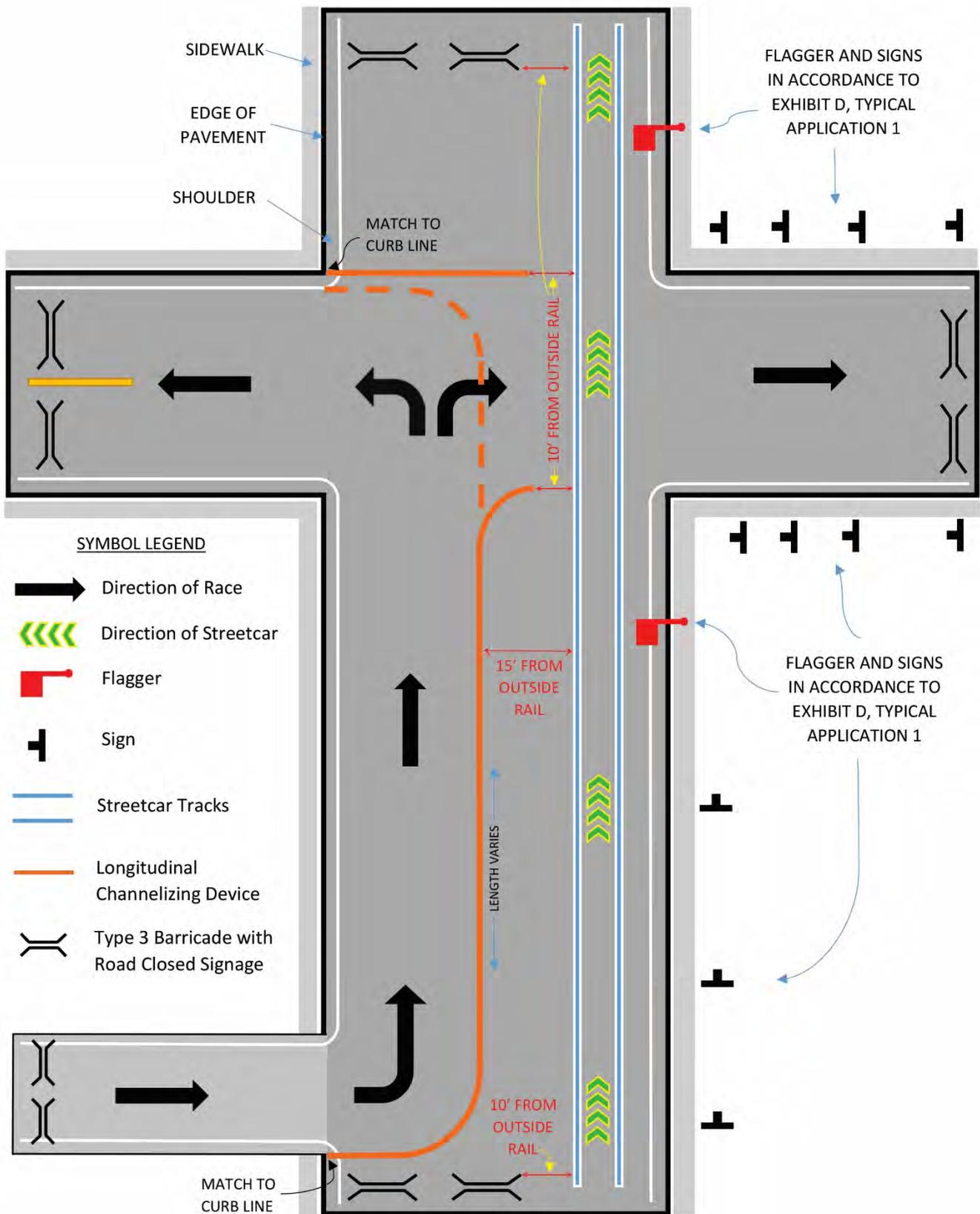
Work Zones Within Vicinity of Streetcar Operational ROW



X – See Table 6C-1

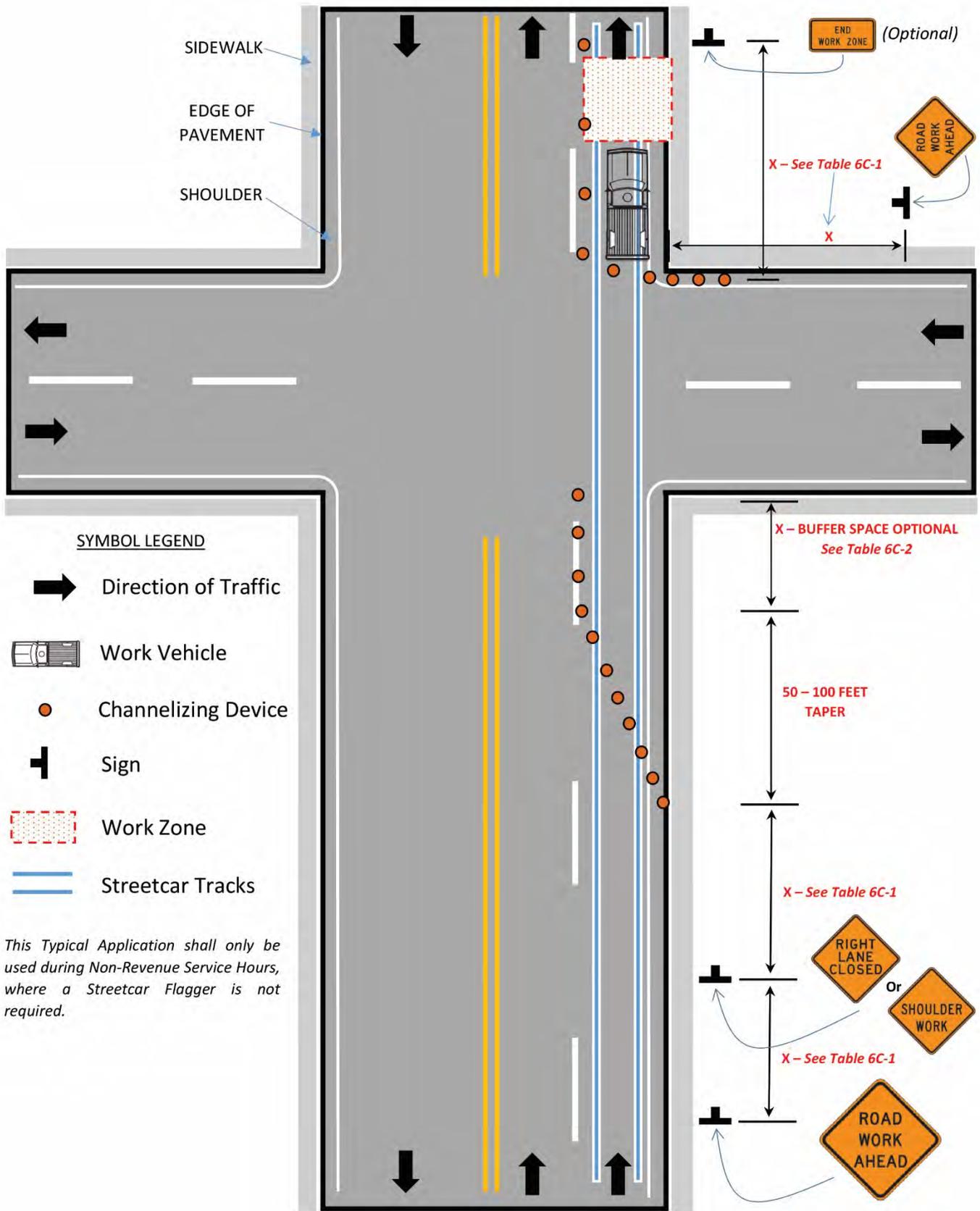
Typical Application 3

Events (Road Races or Pedestrian Traffic Control) Where Travel Path is on Same Roadway as Operational ROW



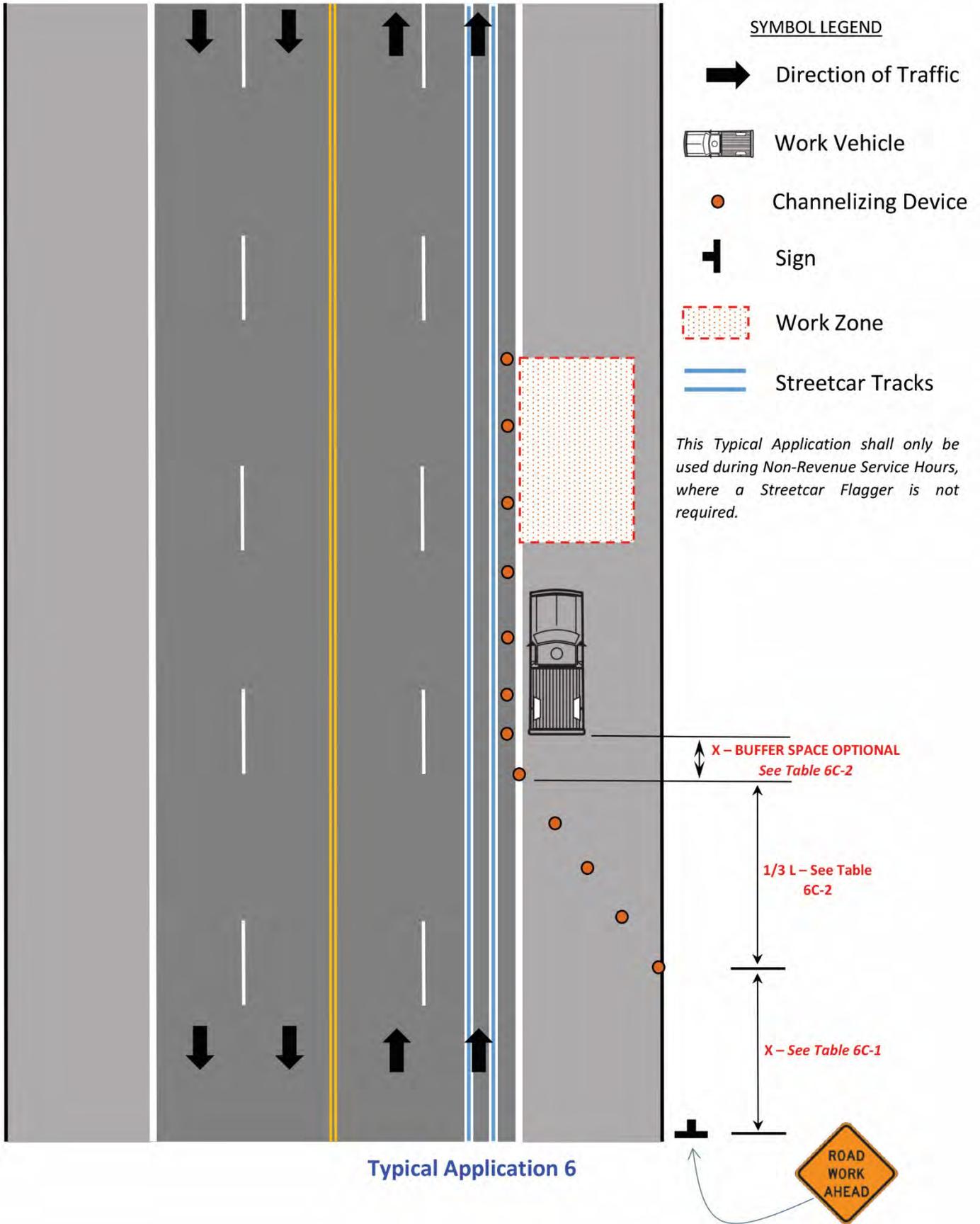
Typical Application 4

Streetcar Lane Closure on the Far Side of an Intersection (Non-Revenue Service Hours)

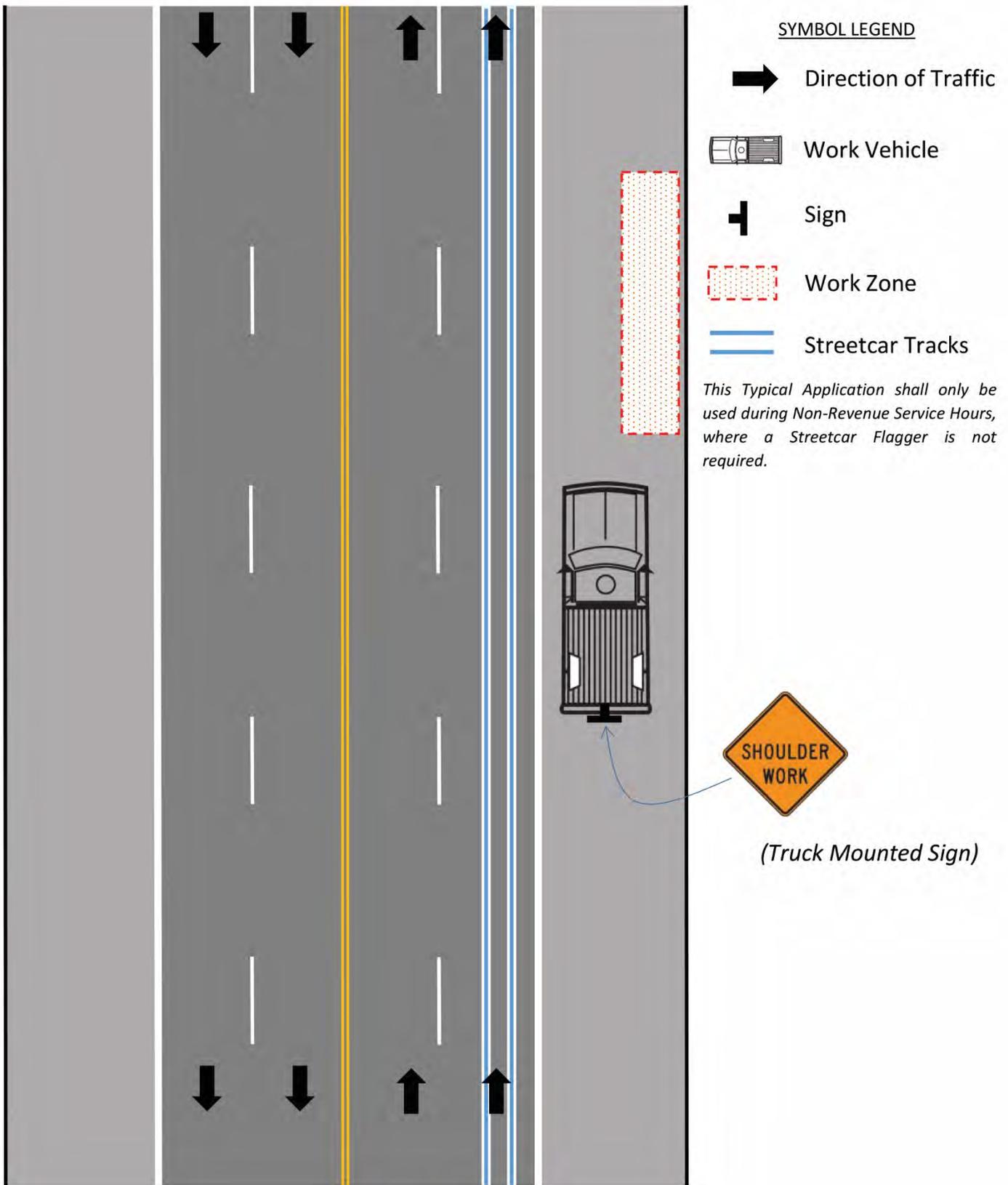


Typical Application 5

Shoulder Work with Minor Encroachment into Streetcar Operational ROW (Non-Revenue Service Hours)



**Short Duration or Mobile Operation on a Shoulder or Sidewalk on Streetcar Operational ROW
(Non-Revenue Service Hours)**



Typical Application 7

Reference Tables

Table 6C-1. Suggested Advance Warning Sign Spacing

Road Classification	Posted Speed (MPH)	Sign Spacing "X" (Feet)
Conventional Highway	25	100
	30	120
	35	160
	40	240
	45	320
	50	400
	55*	500
	60*	600
	65*	700
	70*	800
	75*	900
80*	1000	
Expressway or Freeway	All Speeds	See Typical Applications (Chapter 6H) **

* Distance between signs should be increased to have 1500 feet advance warning (See Section 6C.04.07)

** Distance between signs should be increased to have 1/2 mile or more advance warning. (See Section 6C.04.05)

Table 6C-2. Longitudinal Buffer Space

Speed* (mph)	Length (Feet)
20	40
25	60
30	90
35	120
40	155
45	195
50	240
55	295
60	350
65	410
70	475
75	540
80	615

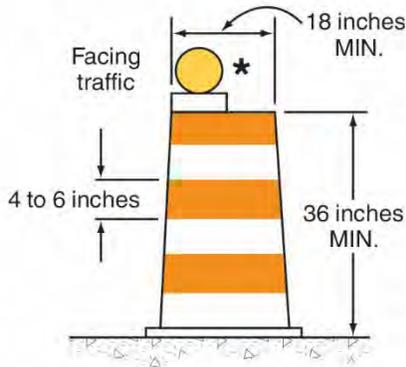
* Posted speed Based upon American Association of state Highway and Transportation Officials (AASHTO) braking distance portion of stopping sight distance for wet and level pavements (A Policy on Geometric Design of Highways and Streets, AASHTO, 2004, P. 112). This AASHTO document also recommends adjustments for the effect of grade on stopping and variation for trucks.

Table 6E-1. Stopping Sight Distance as a Function of Speed

Speed*	Distance
20 mph	115 feet
25 mph	155 feet
30 mph	200 feet
35 mph	250 feet
40 mph	305 feet
45 mph	360 feet
50 mph	425 feet
55 mph	495 feet
60 mph	570 feet
65 mph	645 feet
70 mph	730 feet
75 mph	820 feet
80 mph	910 feet

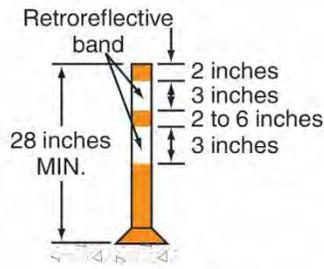
* Posted speed, off-peak 85th-percentile speed prior to work starting, or the anticipated operating speed

Channelizing Devices

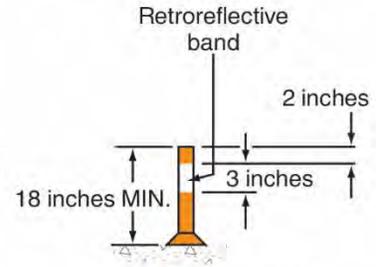


DRUM

* Warning lights (optional)

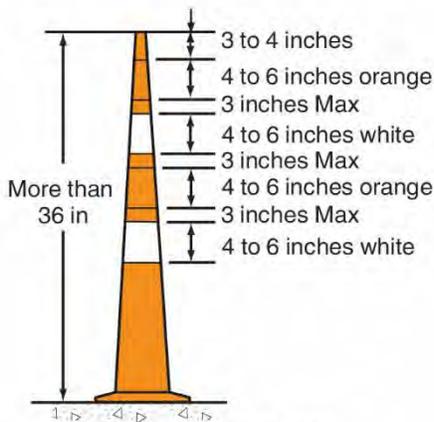


Night and/or freeway
High-speed roadway
(≥ 45 mph)

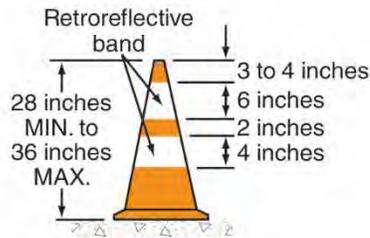


Day and low-speed
roadway (≤ 40 mph)

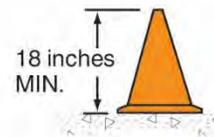
TUBULAR MARKERS



Min. 4 Retroreflective Bands
(2 white and 2 orange)

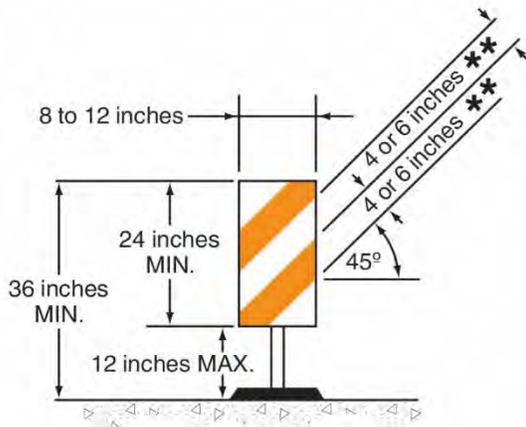


Night and/or freeway
High-speed roadway
(≥ 45 mph)

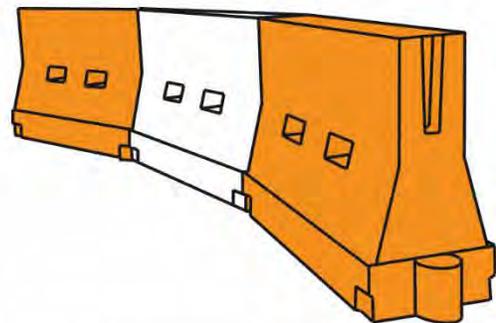


Day and low-speed
roadway (≤ 40 mph)

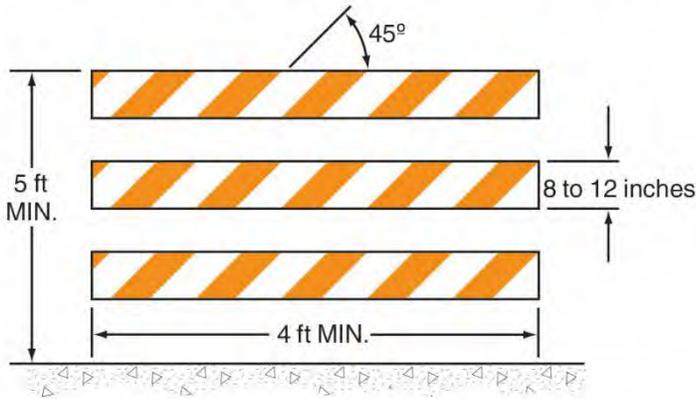
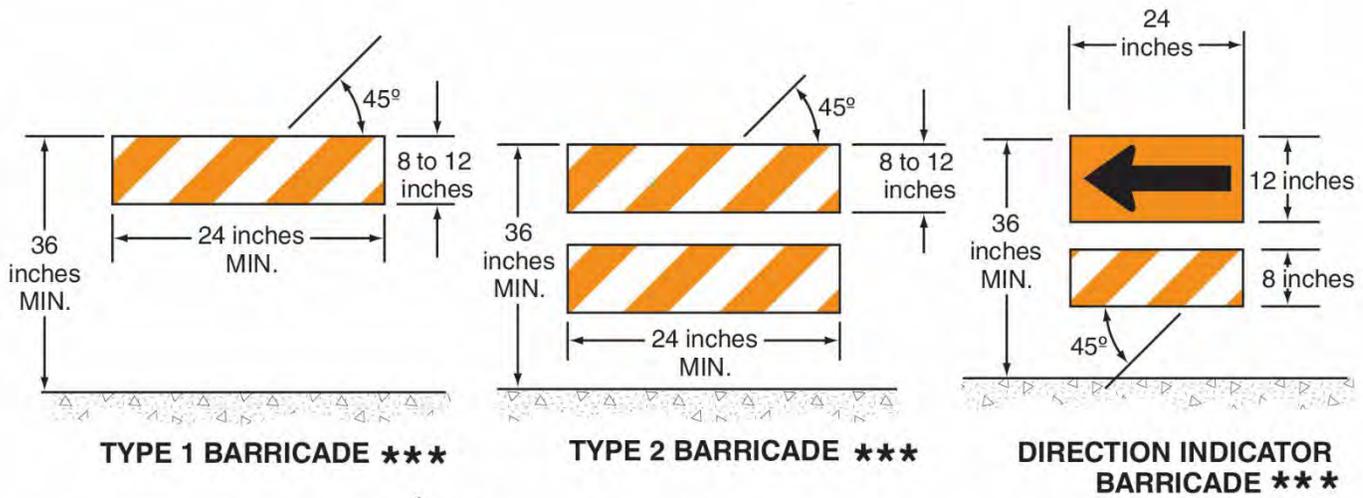
CONES



VERTICAL PANEL



**LONGITUDINAL CHANNELIZING
DEVICE**



*** Rail stripe widths shall be 6 inches, except that 4-inch wide stripes may be used if rail lengths are less than 36 inches. The sides of barricades facing traffic shall have retroreflective rail faces.

Exhibit E

Insurance Requirements



It shall be the policy of El Paso Streetcar to require the following insurances: Railroad Protective Liability Insurance **AND/OR** Commercial General Liability Insurance with a Contractual Liability-Railroads Endorsement prior to the issuance of any Track Access Permit. This requirement will be reviewed and enforced by the City of El Paso’s Risk Management Department.

The following are specific requirements as noted:

FOR CONSTRUCTION, MAINTENANCE, AND SPECIAL EVENT ACTIVITIES:

Railroad Protective Liability (RPL) Insurance Requirements

- A. El Paso Streetcar shall require all Permittees who request access for construction, maintenance, or special events to furnish El Paso Streetcar with a Railroad Protective Liability (RPL) Insurance with the following requirements:

Construction or Maintenance	Special Events with modifications or accommodations to site (i.e. use of machinery near the OCS infrastructure, booths near the tracks, powering off of the system, etc.)	Special Events without modifications or accommodations to site
Policy having a combined single limit of not less than \$1,000,000.00 Bodily Injury / \$1,000,000.00 Property Damage Liability each occurrence and \$6,000,000.00 in the aggregate.	Policy having a combined single limit of not less than \$1,000,000.00 Bodily Injury / \$1,000,000.00 Property Damage Liability each occurrence and \$6,000,000.00 in the aggregate.	Policy having a combined single limit of not less than \$1,000,000.00 Bodily Injury / \$1,000,000.00 Property Damage Liability each occurrence and \$2,000,000.00 in the aggregate.

- B. The Standards for the Railroad Protective Liability Insurance are as follows:
 1. The policy must be written on an Occurrence Form using the Insurance Services Offices (ISO) Railroad Protective Liability Insurance Form, or equivalent insurance agency specific form.
 2. The name insured shall read:

The City of El Paso Texas
Attn: Sun Metro – El Paso Streetcar
10151 Montana Avenue
El Paso, Texas 79925
 3. The description of work or event must appear on the Declarations, and must match the *Description of Work* or *Description of Event* as written on the Track Access Request Form (Exhibit A or A1).

4. The work or event location must appear on the Declarations and must include the city, state, and appropriate highway name/number or street name/number. NOTE: Do not include any references to milepost(s) or addresses on the insurance policy.
 5. The name and address of the Permittee must appear on the Declarations.
 6. The name and address of El Paso Streetcar, as referenced above, must be identified on the Declarations as the **“Involved Governmental Authority.”**
 7. The definitions of “physical damage to property” or “property damage” SHALL be amended to read “Direct and accidental loss or damage to all property of the named insured and to all property in the care, custody, or control of the insured.”
- C. If any part of the work or event is sublet, similar insurance, and evidence thereof as specified above, shall be provided by or on behalf of the Permittee to cover its work or event on El Paso Streetcar’s Right-of-Way.
- D. Prior to entry onto El Paso Streetcar Right-of-Way, the Railroad Protective Liability Insurance Policy shall be submitted by Permittee to El Paso Streetcar. In addition, certificates of insurance evidencing the Permittees and any Sub-Permittees Commercial General Liability Insurance shall be issued to El Paso Streetcar at the address below. The certificates of insurance shall state that the insurance coverage will not be suspended, voided, cancelled, or reduced in coverage or limits without (30) days advanced written notice to El Paso Streetcar.

ADDRESS:

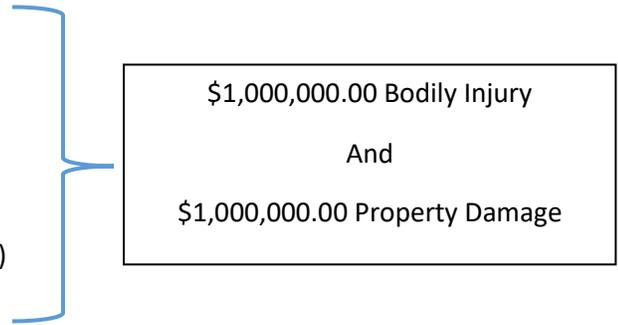
Sun Metro – El Paso Streetcar
Attn: Planning Department – ROW Compliance
10151 Montana Avenue
El Paso, Texas 79925

FOR CONSTRUCTION OR MAINTENANCE ACTIVITY WHERE EPSC CONTRACTS DIRECTLY WITH PERMITTEE FOR SERVICES OR SPECIAL 3RD PARTY CIRCUMSTANCES:

Commercial General Liability with Contractual Liability-Railroads Endorsement

- A. El Paso Streetcar shall require all Permittees to furnish El Paso Streetcar with a Commercial General Liability (CGL) Insurance Policy **WITH a CONTRACTUAL LIABILITY – RAILROADS ENDORSEMENT** having a combined single limit of not less than \$1,000,000.00 Bodily Injury / \$1,000,000.00 Property Damage Liability each occurrence. This coverage and endorsement is only required when EPSC contracts directly with a permittee to perform work on any Streetcar system(s) or infrastructure. This coverage may also be required under special circumstances where, due to the nature of the permittees proposed work may cause significant risk to Streetcar system(s) and/or infrastructure.
- B. The Standards for the Commercial General Liability Insurance are as follows:
1. The policy must be written on an Occurrence Form using the Insurance Services Offices (ISO) Commercial General Liability Insurance Form, or equivalent insurance agency specific form, and contain the following coverage subject to the below stated limits per occurrence:

- a. Premises/operations liability
- b. Products/completed operations
- c. Personal/advertising injury
- d. Contractual liability
- e. Broad-form property damage
- f. Independent contractor liability
- g. Explosion, Collapse and Underground (XCU)
- h. Cyber Liability/Data/Breach/Ransom*



In addition, the following coverage limits per occurrence shall apply:

- a. Automobile Liability (\$1,000,000.00 Bodily Injury / \$500,000.00 Property Damage)
- b. Worker’s Compensation (Statutory Coverage / \$500,000.00 Employers Liability)
- c. Professional (Errors & Omissions) Liability (If required) (\$1,000,00.00)
- d. Excess Liability Insurance (if required) (\$5,000,000.00)

***Cyber Liability/Data/Breach/Ransom endorsement is required if the scope of work includes:**

- **Software/Hardware installation or updates to any computer based or electronic device for storing and processing data.**
- **Electronic devices that control any component of the streetcar system.**
- **Access to electronic data, programs or systems currently used by the City of El Paso.**
- **All contracts/agreements will be reviewed on a case-by-case basis for applicability.**

2. The policy SHALL include a Contractual Liability-Railroads endorsement subject to the following limits per occurrence: (\$1,000,000.00 Bodily Injury / \$1,000,000.00 Property Damage).

3. The name insured shall match that of the Permittee as listed in Exhibit A or A1 of the Track Access Program Policy.

4. The certificate holder shall read:

Sun Metro – El Paso Streetcar
Attn: Planning Department – ROW Compliance
10151 Montana Avenue
El Paso, Texas 79925

5. The description of work or event must appear on the Declarations, and must match the *Description of Work* or *Description of Event* as written on the Track Access Request Form (Exhibit A or A1).

6. The work or event location must appear on the Declarations and must include the city, state, and appropriate highway name/number or street name/number. NOTE: Do not include any references to milepost(s) or addresses on the insurance policy.

7. The name and address of the Permittee must appear on the Declarations.

8. The City, its officials, employees, agents and contractors shall be named as additional insured and contain a “blanket waiver of subrogation” clause in favor of the City. The named insured for the City shall be as follows:

The City of El Paso Texas
Attn: Sun Metro – El Paso Streetcar
10151 Montana Avenue
El Paso, Texas 79925

C. If any part of the work or event is sublet, similar insurance, and evidence thereof as specified above, shall be provided by or on behalf of the Permittee to cover its work or event on El Paso Streetcar’s Right-of-Way.

D. Prior to entry onto El Paso Streetcar Right-of-Way, the Commercial General Liability Insurance Policy with Contractual Liability-Railroads Endorsement shall be submitted by Permittee to El Paso Streetcar. In addition, certificates of insurance evidencing the Permittees and any Sub-Permittees Commercial General Liability Insurance shall be issued to El Paso Streetcar at the address below. The certificates of insurance shall state that the insurance coverage will not be suspended, voided, cancelled, or reduced in coverage or limits without (30) days advanced written notice to El Paso Streetcar.

ADDRESS:

Sun Metro – El Paso Streetcar
Attn: Planning Department – ROW Compliance
10151 Montana Avenue
El Paso, Texas 79925

ADDITIONAL NOTES:

The Permittee and their subcontractors' insurance coverage shall be primary insurance as respects to the City, its officials, employees, agents and contractors. Any insurance or self-insurance maintained by the City, its officials, employees, agents and contractors shall be in excess of the Permittees or Permittees' subcontractor's insurance and shall not contribute to the Permittees or Permittees' subcontractor's insurance.

Prior to undertaking any work or event as permitted by the El Paso Streetcar, at no expense to the City, shall furnish the City, at the address below, a certificate of insurance with original endorsements affecting coverage for each of the insurance policies provided in this exhibit. Any deductibles or self-insured retentions must be declared to, and approved by the City.

ADDRESS:

The City of El Paso Texas
Attn: Sun Metro – El Paso Streetcar
10151 Montana Avenue
El Paso, Texas 79925

OR UPLOAD VIA THE CITY OF EL PASO ONLINE PERMITTING SYSTEM:

<https://epermits.elpasotexas.gov/CitizenAccess/Default.aspx>

**NO WORK OR EVENT WILL BE PERMITTED BY EL PASO STREETCAR ON ITS RIGHT-OF-WAY
UNTIL IT HAS REVIEWED AND APPROVED THE EVIDENCE OF INSURANCE REQUIRED
HEREIN.**

Exhibit F

Permit Requirements



Issuance of Track Access Permit

Once all requirements have been satisfied, El Paso Streetcar will issue a Track Access Permit to the Permittee. Track Access Permits will be issued in accordance to the following categories:

1. Construction
2. Special Event
3. Maintenance
4. Emergency

Please see Section 2 of the Track Access Program Policy for definitions of each category.

Upon the issuance of a Track Access Permit, the Permittee shall:

- Abide by all conditions imposed by the Track Access Program.
- Abide by all special conditions or notices imposed by the Track Access Permit.
- Keep a copy of approved permit, application, work/site plan, traffic control plan, or other approved document on the person, or work site. The permit must be available for inspection by El Paso Streetcar or other City or regulatory official.

In the event the Permittee cannot execute the permitted activity due to inclement weather, emergency, equipment malfunction, acts(s) of nature or any other issue that is outside the control of the permittee, the permittee may contact EPSC to reschedule the permitted activity, subject to the availability of ROW allocation and operational considerations. EPSC will waive the minimum 14/30/45 calendar day requirement on a case-by-case basis if the above-mentioned conditions are satisfied.

Failure of Permittee or sub-contractor to provide sufficient material to conduct work shall not be sufficient cause to waive the minimum calendar day requirement.

Failure of traffic control provider to submit for a Traffic Control Permit request in a timely manner shall not be sufficient cause to waive the minimum calendar day requirement.

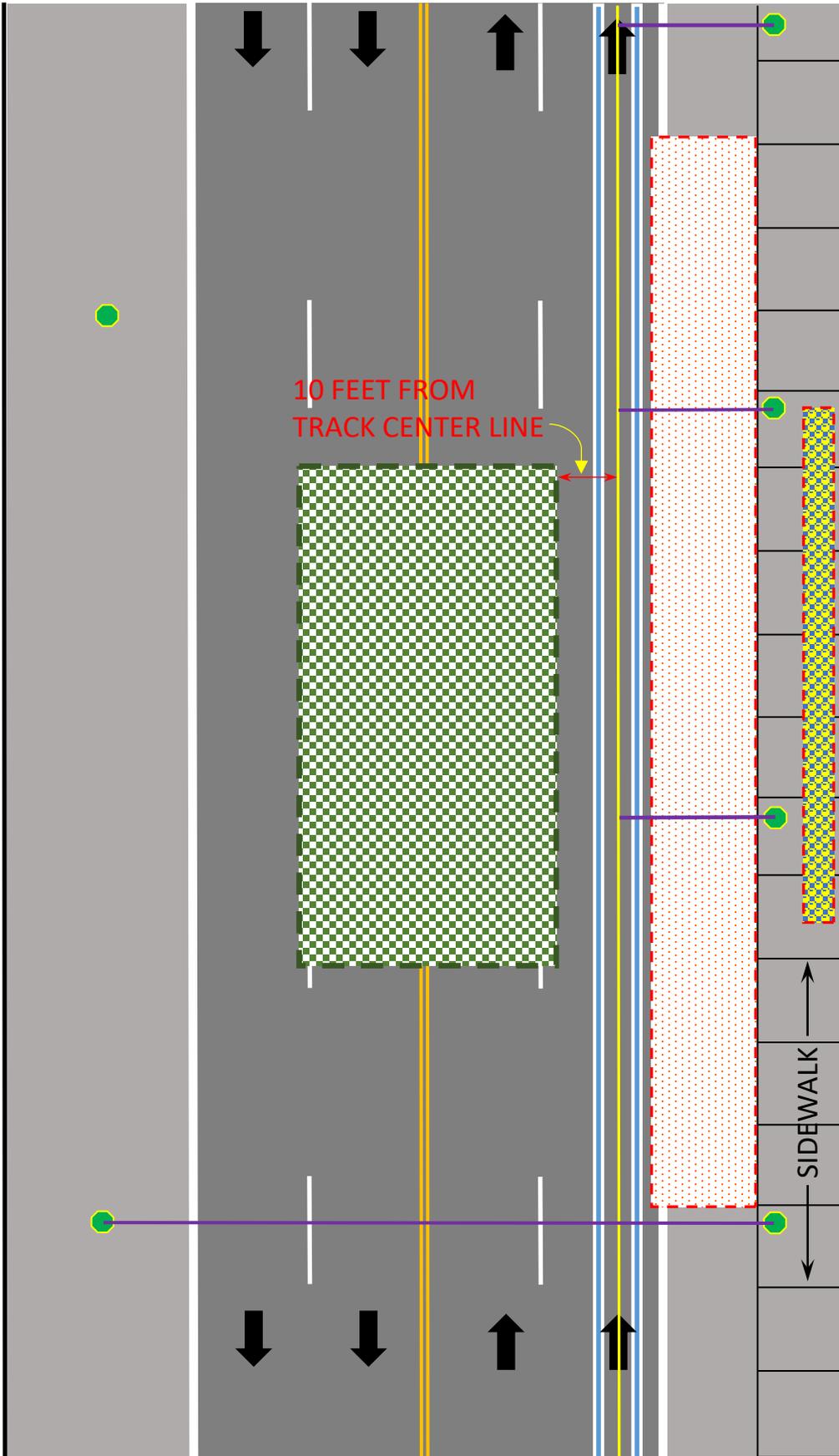
Permit Fees

At this time, El Paso Streetcar does not charge a fee for the issuance of a Track Access Permit, but reserves the right to impose a fee schedule at a later date.

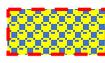
Exhibit G

Crane Staging and Placement Standards





SYMBOL LEGEND

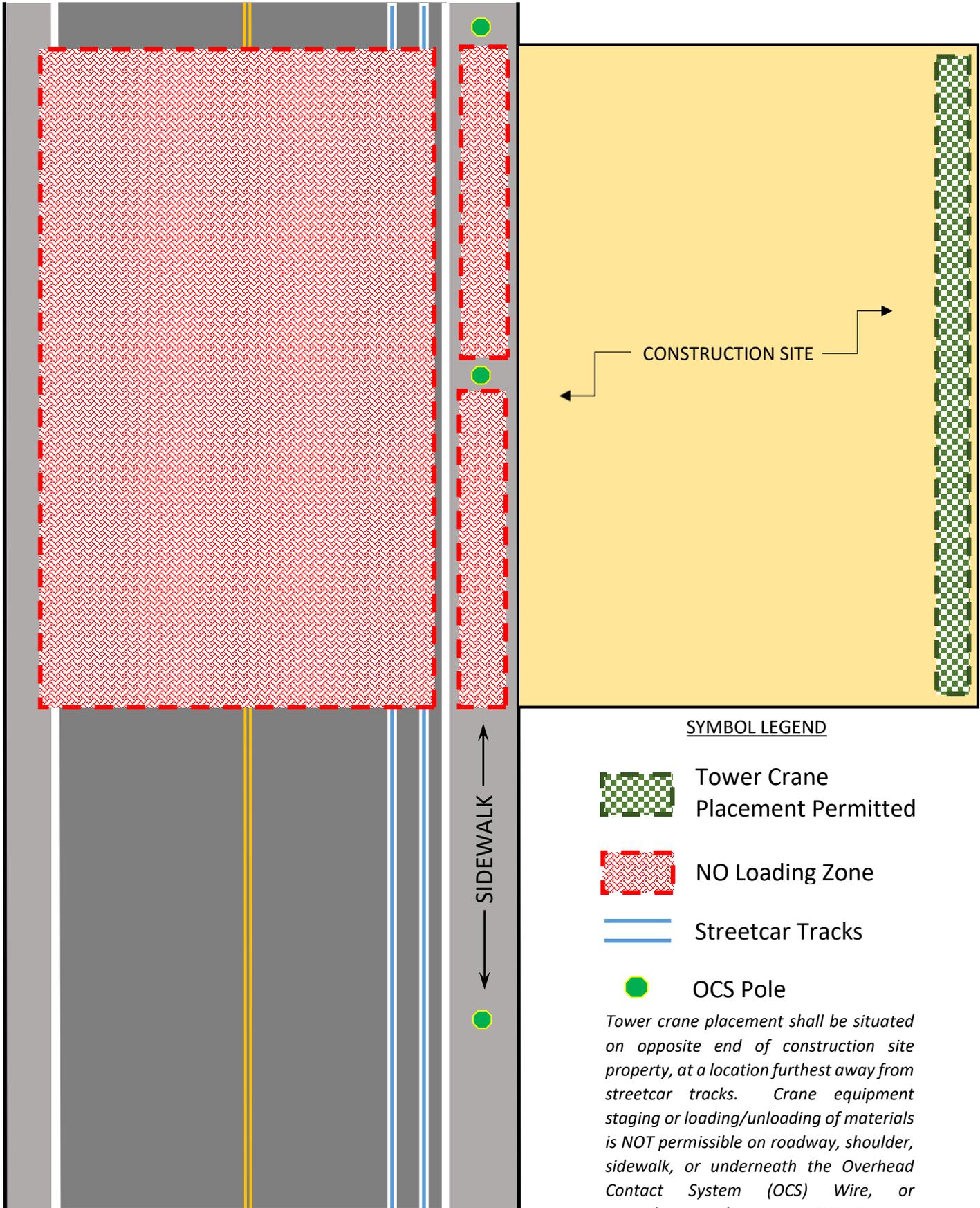
-  Direction of Traffic
-  Crane Staging Permissible
-  Crane Staging **NOT** Permissible
-  Streetcar Tracks
-  OCS Wire
-  OCS Pole
-  OCS Wire Appurtenance
-  Scissor Lift Permissible*

Crane staging shall only be permissible in the GREEN hatched area. Staging area size and location varies. Crane staging is NOT permissible on roadway shoulder closest to tracks, underneath the Overhead Contact System (OCS) Wire, or under/between any OCS Pole or Appurtenance.

** = Scissor Lift Permissible on sidewalk ONLY under certain circumstances on a case-by-case basis, subject to the approval of El Paso Streetcar.*

Crane shall also mean any aerial lift system or vehicle capable of carrying people or equipment.

CRANE STAGING ZONES



SYMBOL LEGEND

 Tower Crane Placement Permitted

 NO Loading Zone

 Streetcar Tracks

 OCS Pole

Tower crane placement shall be situated on opposite end of construction site property, at a location furthest away from streetcar tracks. Crane equipment staging or loading/unloading of materials is NOT permissible on roadway, shoulder, sidewalk, or underneath the Overhead Contact System (OCS) Wire, or under/between/over any OCS Pole or Appurtenance.

TOWER CRANE PLACEMENT STANDARDS

Exhibit H

Trenching Standards



SUBMITTAL REQUIREMENTS

Streetcar Zone of Influence

El Paso Streetcar has identified minimum trench safety and excavation requirements for all proposed open trenching or excavations within 25 feet of the track centerline (Streetcar Zone of Influence). Specific design requirements are detailed in Figure H – 1.

General

All drawings and calculations for open trenching shall be prepared, sealed and signed by a Professional Engineer (civil or structural) currently licensed in the State of Texas who has previous experience in the design of trench safety applications.

The designer will be responsible for the accuracy of all controlling dimensions as well as the selection of appropriate materials and systems that accurately reflect the actual field conditions. No open trenching installation within the Streetcar Zone of Influence (25 feet from track centerline) will be allowed until the drawings and calculations are reviewed and accepted by EPSC.

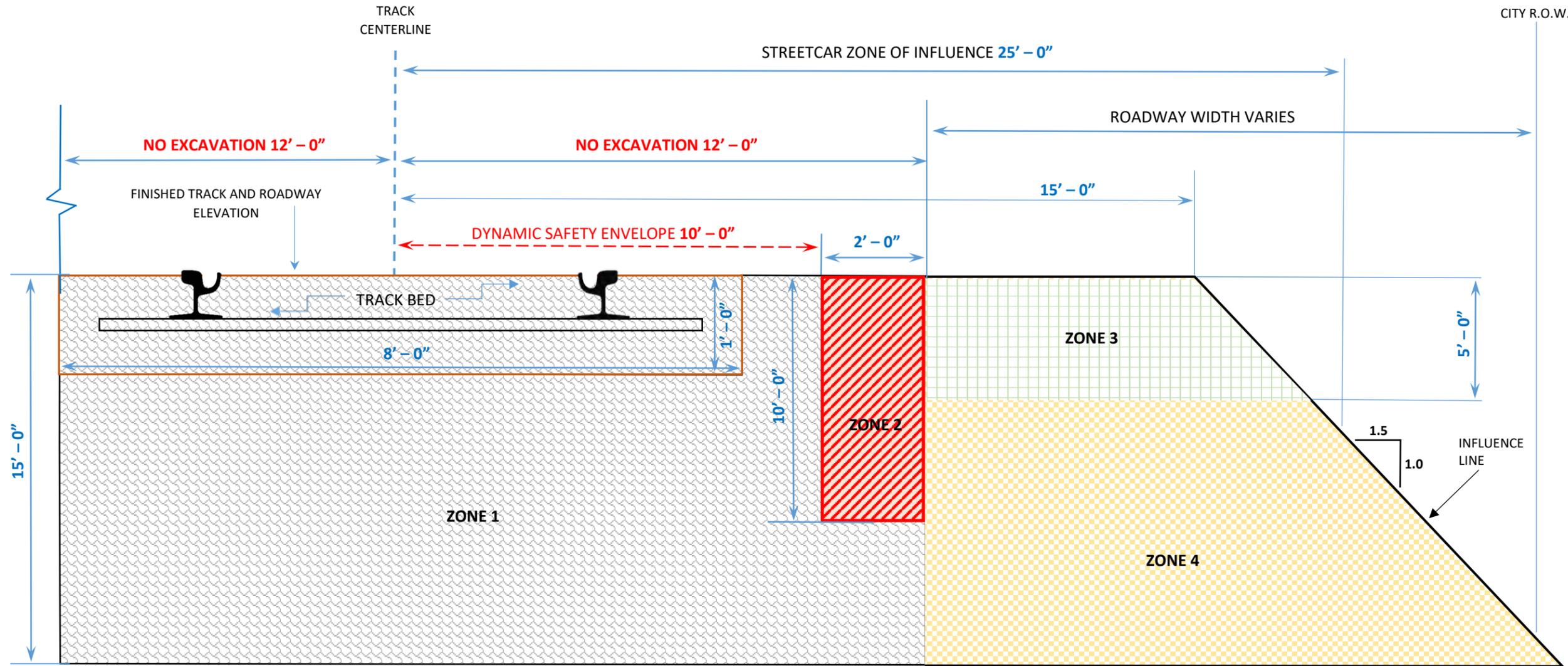
Submittal of forms, drawings and calculations shall be provided to EPSC for review in electronic format transmitted by email, or mail (with CD-R or DVD-R properly labeled). Files shall be Adobe PDF compatible. Each separate document shall be a separate PDF file (drawings, specifications, calculations, forms, etc.).

All submittals, design calculations, specifications and drawings shall be prepared in accordance with a QA/QC process. The QA/QC process may follow the established program of EPSC, Engineer in Responsible Charge firm, or Permittee. At a minimum, the QA/QC process must consist of an independent check of design calculations and an independent QC review of the drawings and specifications prior to submittal to EPSC by qualified individuals. Documentation of the QA/QC process, including names and contact information of independent reviewers, shall be made available to EPSC at their request.

A minimum of **FIFTEEN (15) CALENDAR DAYS** should be allowed for EPSC's review, provided that all required submittal materials are included and properly identified.

Requirements

Specific requirements concerning open trenching are detailed in Exhibit H1 – Excavation Support Guidelines which are available for download on Sun Metro's website www.sunmetro.net/streetcar.



TYPICAL SECTION VIEW – STREETCAR TRENCH ZONES– SCALE: N.T.S.

GENERAL NOTES:

1. CROSS – SECTION SHOWN IS TYPICAL OF FIELD CONDITIONS FOUND ON STREETCAR OPERATIONAL RIGHT-OF-WAY (ALIGNMENT).
2. A CONCRETE TRACK BED TYPICALLY MEASURING 8 FEET IN WIDTH AND 12 INCHES IN DEPTH (FROM FINISHED ROADWAY ELEVATION) IS FOUND THROUGHOUT THE ALIGNMENT.
3. HMAC OR CONCRETE ROADWAY PAVEMENT OR A COMBINATION OF BOTH MAY BE FOUND ON EITHER SIDE OF TRACK BED AT VARIOUS DEPTHS.
4. AN 8 INCH COMPACTED BASE IS TYPICAL UNDERNEATH THE ADJACENT HMAC OR CONCRETE PAVEMENT.
5. A 12 INCH COMPACTED BASE IS TYPICAL UNDERNEATH THE TRACK BED.
6. SUBGRADE DEPTH VARIES THROUGHOUT THE ALIGNMENT.
7. **ZONE 1** – EXCAVATION IS PROHIBITED.
8. **ZONE 2** – NO EXCAVATION OR TEMPORARY SHORING INSTALLATION WILL BE ALLOWED WITHOUT THE SPECIAL WRITTEN PERMISSION OF EL PASO STREETCAR (EPSC). ALTERNATES TO SHORED EXCAVATIONS SHALL BE UTILIZED WHEN PRACTICAL. IF EPSC GRANTS A VARIANCE TO ALLOW EXCAVATION, VERTICAL EXCAVATION WITH CONTINUOUS SHORING WALLS IS REQUIRED. SHORING INSTALLATION SHALL BE COMPLETE PRIOR TO ANY EXCAVATION. DESIGN OF THE SHORING SYSTEM SHALL INCLUDE LATERAL SURCHARGE DUE TO STREETCAR LIVE LOAD.
9. **ZONE 3** – EXCAVATION REQUIRES TEMPORARY SHORING. LATERAL SURCHARGE DUE TO STREETCAR LIVE LOAD NEED NOT BE CONSIDERED IN SHORING DESIGN. THE EXCAVATION SHALL BE PROVIDED WITH A SHORING SYSTEM THAT ACTIVELY SUPPORTS THE SIDES OF THE EXCAVATION AND PREVENTS THE EXCAVATION FACES FROM UNRAVELING OR MOVING. SLOPED EXCAVATIONS ARE NOT PERMITTED.
10. **ZONE 4** – EXCAVATION REQUIRES TEMPORARY SHORING. EXCAVATIONS SHALL BE VERTICAL. CONTINUOUS SHORING WALLS INSTALLED PRIOR TO ANY EXCAVATION ARE PREFERRED. MAXIMUM EXCAVATION LIFTS SHALL BE LIMITED TO FIVE (5) FEET FOR EACH STAGE OF EXCAVATION. **DESIGN OF THE SHORING SYSTEM SHALL INCLUDE LATERAL SURCHARGE DUE TO STREETCAR LIVE LOAD.**
11. **EXCAVATIONS BEYOND INFLUENCE LINE** – LATERAL SURCHARGE DUE TO STREETCAR LIVE LOAD NEED NOT BE CONSIDERED IN THE SHORING DESIGN. SHORED EXCAVATIONS ARE PREFERRED. SLOPED EXCAVATIONS ARE DISCOURAGED. EPSC MAY REQUIRE SLOPE STABILITY ANALYSIS FOR SLOPED EXCAVATIONS.
12. ALL TRENCHES SHALL BE SHORED PER OSHA STANDARDS.
13. ONLY VERTICAL EXCAVATION IS PERMITTED (NO SLOPING CUTS).
14. ALL DRAWINGS AND CALCULATIONS FOR TEMPORARY SHORING SHALL BE PREPARED, SEALED, AND SIGNED BY A PROFESSIONAL ENGINEER (CIVIL OR STRUCTURAL) CURRENTLY LICENSED IN THE STATE OF TEXAS WHO HAS EXPERIENCE IN THE DESIGN OF TEMPORARY SHORING SYSTEMS OF THE TYPE BEING SUBMITTED. THE DESIGNER WILL BE RESPONSIBLE FOR THE ACCURACY OF ALL CONTROLLING DIMENSIONS AS WELL AS THE SELECTION OF SOIL DESIGN VALUES THAT ACCURATELY REFLECT THE ACTUAL FIELD CONDITIONS.

FIGURE
H-1



STREETCAR TRENCH ZONES

DRAWN BY: IMV CHECKED BY: FB APPROVED BY: CI 03/12/2019



Exhibit I

Vertical Clearance Standards



SUBMITTAL REQUIREMENTS

Vertical Clearance

El Paso Streetcar has identified minimum vertical clearance requirements for all proposed aerial crossings over streetcar infrastructure. Specific design requirements are detailed in Figure I – 1.

General

All drawings and calculations for aerial crossings shall be prepared, sealed and signed by a Professional Engineer (civil or electrical) currently licensed in the State of Texas who has previous experience in the design of the particular aerial system being proposed.

The designer will be responsible for the accuracy of all controlling dimensions as well as the selection of appropriate materials and systems that accurately reflect the actual field conditions. No aerial installation within the operational right-of-way will be allowed until the drawings and calculations are reviewed and accepted by EPSC.

Submittal of forms, drawings and calculations shall be provided to EPSC for review in electronic format transmitted by email, or mail (with CD-R or DVD-R properly labeled). Files shall be Adobe PDF compatible. Each separate document shall be a separate PDF file (drawings, specifications, calculations, forms, etc.).

All submittals, design calculations, specifications and drawings shall be prepared in accordance with a QA/QC process. The QA/QC process may follow the established program of EPSC, Engineer in Responsible Charge firm, or Permittee. At a minimum, the QA/QC process must consist of an independent check of design calculations and an independent QC review of the drawings and specifications prior to submittal to EPSC by qualified individuals. Documentation of the QA/QC process, including names and contact information of independent reviewers, shall be made available to EPSC at their request.

A minimum of **FIVE (5) CALENDAR DAYS** should be allowed for EPSC's review, provided that all required submittal materials are included and properly identified.

Drawings

The aerial crossing drawings must be complete and shall accurately describe the nature of the work. Drawings shall be to scale.

At a minimum, the drawings shall include the following:

- Plan view that includes the following information and meets the following criteria:
 - Streetcar alignment centerline
 - North Arrow
 - All pertinent topographic information
 - Labeling and identification of all Streetcar Operating System elements and facilities (rails, track bed, track centerline, signals, overhead contact system (OCS) poles, OCS wires, and OCS appurtenances)
 - Labeling and identification of all proposed aerial crossings and facilities (utility poles, structures, wires, support elements, etc.)

- Section view normal to the track(s) that includes the following information:
 - Length of proposed aerial crossing span between pole 1 and pole 2 (see legend label **X**) as depicted in Figure I -1
 - Height of Pole 1 (see legend label **Y1**) as depicted in Figure I – 1
 - Height of attachment point on Pole 1 (see legend label **Y2**) as depicted in Figure I – 1
 - Height of Pole 2 (see legend label **Y3**) as depicted in Figure I – 1
 - Height of attachment point on Pole 2 (see legend label **Y4**) as depicted in Figure I – 1
 - Height of proposed conductor from top of OCS wire (see label **C1**) as depicted in Figure I – 1
 - Height of proposed conductor from top of OCS pole (see legend label **C2**) as depicted in Figure I – 1 if aerial path is crossing directly above OCS pole

- Specification sheet of cable or proposed material that includes the following information:
 - General specifications
 - Construction materials
 - Dimensions
 - Physical specifications
 - Flame test specifications (if applicable)
 - Environmental specifications
 - Mechanical test specifications
 - Environmental test specifications

- Typical details shall only include those that are pertinent to aerial installations only. Details including any reference to directional boring or trenching shall not be included.

NOTES:

1. ALL CLEARANCES ARE MINIMUM.
2. VERTICAL CLEARANCES APPLY UNDER ALL WEATHER CONDITIONS:
 - a. CONDUCTOR TEMPERATURE RANGE OF -20° TO 120° F, NO WIND, WITH FINAL UNLOADED SAG IN THE WIRE.
3. FOR VOLTAGES EXCEEDING 22KV (UP TO 470KV) THE CLEARANCE SHALL BE INCREASED BY 0.4 INCHES FOR EACH 1KV, OR FRACTION THEREOF.
4. VERTICAL CLEARANCES TO NON-OCS CONDUCTORS APPLY UNDER THE FOLLOWING CONDITIONS WHICHEVER PRODUCES THE LARGEST SAG IN THE CONDUCTOR:
 - a. CONDUCTOR SAG AT 120° F, NO WIND DISPLACEMENT, OR;
 - b. MAXIMUM CONDUCTOR DESIGNED OPERATING TEMPERATURE IF GREATER THAN 120° F, OR;
 - c. 32° F, WITH RADIAL ICE OF 0.25 INCHES.
5. ALL ELECTRICAL CLEARANCES SHALL COMPLY WITH NATIONAL ELECTRICAL SAFETY CODE (NEC). AT LOCATIONS WHERE THERE ARE DISCREPANCIES BETWEEN NESC ICE THICKNESS VALUES FOR CLEARANCE AND STRUCTURAL LOADS, THE MORE CONSERVATIVE VALUE FOR THE APPLICATION SHALL BE USED.
6. NON-OCS EQUIPMENT SHALL NOT BE WITHIN 10 FEET OF OCS EQUIPMENT. NO GROUNDED ITEM SHALL BE WITHIN 4 FEET OF THE OCS WITHOUT ELECTRICAL SHEETING PER SPECIFICATION.
7. TRACK BED MAY BE PRESENT IN ANY LANE WITHIN THE CITY R.O.W.
8. OCS POLES ARE LOCATED ON BOTH SIDES OF THE STREET.
9. OCS POLES WITH CROSS SPAN CABLES ARE TYPICALLY LOCATED AT TURNS ON THE ALIGNMENT BUT MAY BE PRESENT AT OTHER LOCATIONS WHERE NECESSARY.

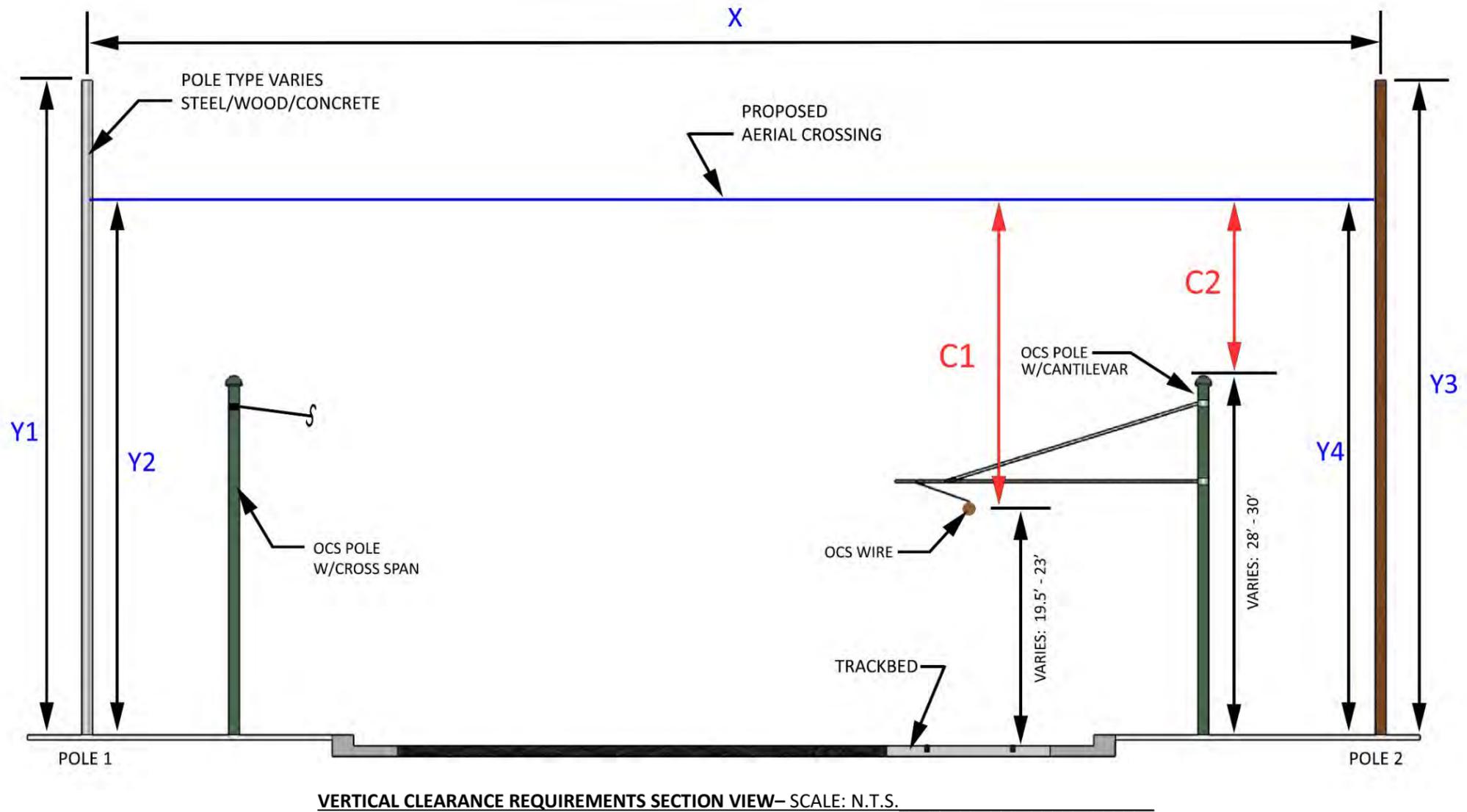
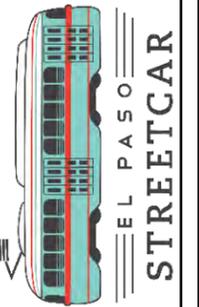


FIGURE 1-1



STREETCAR OVERHEAD CONTACT SYSTEM (OCS) WIRE/POLE VERTICAL CLEARANCES

12/12/2019

OCS WIRE AND POLE CLEARANCE REQUIREMENTS						
LEGEND	GUYS & SPAN WIRES	COMMON WIRES, CABLES & MESSengers	LIGHTING PROTECTION WIRES	SUPPLY LINES 0V - 750V	SUPPLY LINES 750V - 22KV	SUPPLY LINES GREATER THAN 22KV
C1 - MINIMUM CLEARANCE ABOVE OCS WIRE	4' - 0"	4' - 0"	4' - 0"	4' - 0"	6' - 0"	NOTE 3
C2 - MINIMUM CLEARANCE ABOVE OCS POLE	10' - 0"					

LEGEND

- X = LENGTH OF SPAN BETWEEN POLE 1 AND POLE 2
- Y1 = HEIGHT OF POLE 1
- Y2 = HEIGHT OF ATTACHMENT POINT ON POLE 1
- Y3 = HEIGHT OF POLE 2
- Y4 = HEIGHT OF ATTACHMENT POINT ON POLE 2



Exhibit J

Directional Boring Standards



SUBMITTAL REQUIREMENTS

Directional Bore Clearance

El Paso Streetcar has identified minimum directional bore clearance requirements for all proposed subterranean crossings under streetcar infrastructure. Specific design requirements are detailed in Figure J – 1.

General

All drawings and calculations for directional boring shall be prepared, sealed and signed by a Professional Engineer (civil or structural) currently licensed in the State of Texas who has previous experience in the design and placement of subterranean facilities.

The designer will be responsible for the accuracy of all controlling dimensions as well as the selection of appropriate materials and systems that accurately reflect the actual field conditions. No subterranean installation within the operational right-of-way will be allowed until the drawings and calculations are reviewed and accepted by EPSC.

Submittal of forms, drawings and calculations shall be provided to EPSC for review in electronic format transmitted by email, or mail (with CD-R or DVD-R properly labeled). Files shall be Adobe PDF compatible. Each separate document shall be a separate PDF file (drawings, specifications, calculations, forms, etc.).

All submittals, design calculations, specifications and drawings shall be prepared in accordance with a QA/QC process. The QA/QC process may follow the established program of EPSC, Engineer in Responsible Charge firm, or Permittee. At a minimum, the QA/QC process must consist of an independent check of design calculations and an independent QC review of the drawings and specifications prior to submittal to EPSC by qualified individuals. Documentation of the QA/QC process, including names and contact information of independent reviewers, shall be made available to EPSC at their request.

A minimum of **FIVE (5) CALENDAR DAYS** should be allowed for EPSC's review, provided that all required submittal materials are included and properly identified.

Drawings

The directional boring drawings must be complete and shall accurately describe the nature of the work. Drawings shall be to scale.

At a minimum, the drawings shall include the following:

- Plan view that includes the following information and meets the following criteria:
 - Streetcar alignment centerline
 - North Arrow
 - All pertinent topographic information
 - Labeling and identification of all Streetcar Operating System elements and facilities (rails, track bed, track centerline, signals, overhead contact system (OCS) poles, OCS wires, and OCS appurtenances)
 - Labeling and identification of all proposed directional borings and support facilities (pot holes, bore pits, etc.)

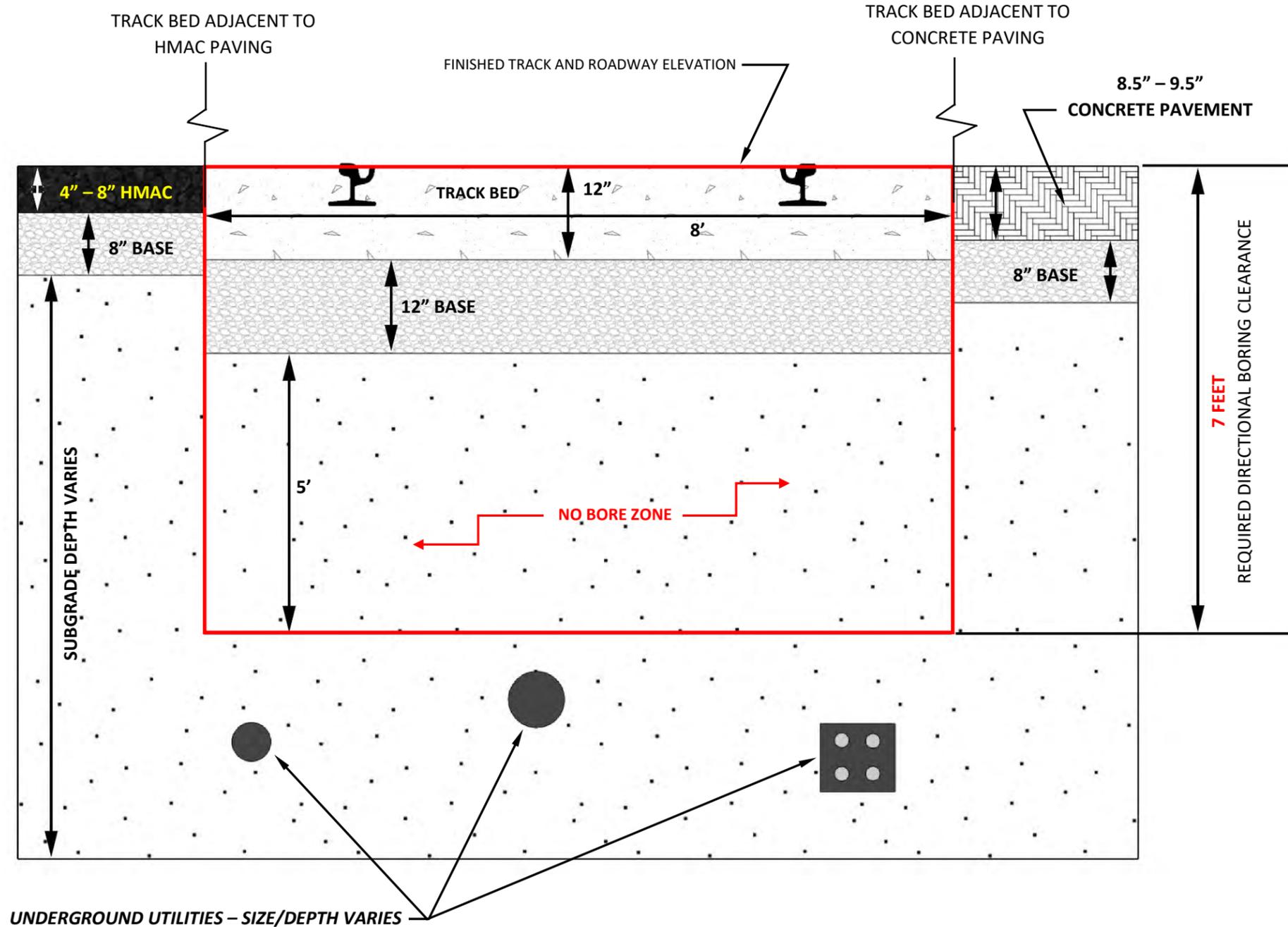
- Section view normal to the track(s) that includes the following information:
 - Length of proposed bore path
 - Depth of proposed bore path
 - Labeling of “No Bore Zone” as depicted in Figure J – 1
 - Direction of bore path
 - Depth of underground utilities (if known)

- Specification sheet of proposed material that includes the following information:
 - General specifications
 - Construction materials
 - Dimensions
 - Physical specifications
 - Flame test specifications (if applicable)
 - Environmental specifications
 - Mechanical test specifications
 - Environmental test specifications

- Typical details shall only include those that are pertinent to directional boring installations only. Details including any reference to aerial applications or trenching (except those identifying bore/receiving pits) shall not be included.

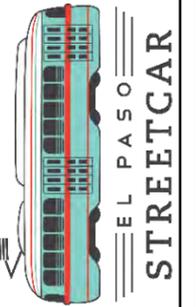
GENERAL NOTES:

1. CROSS – SECTION SHOWN IS TYPICAL OF FIELD CONDITIONS FOUND ON STREETCAR OPERATIONAL RIGHT-OF-WAY (ALIGNMENT).
2. A CONCRETE TRACK BED TYPICALLY MEASURING 8 FEET IN WIDTH AND 12 INCHES IN DEPTH (FROM FINISHED ROADWAY ELEVATION) IS FOUND THROUGHOUT THE ALIGNMENT.
3. HMAC OR CONCRETE ROADWAY PAVEMENT OR A COMBINATION OF BOTH MAY BE FOUND ON EITHER SIDE OF TRACK BED AT VARIOUS DEPTHS.
4. AN 8 INCH COMPACTED BASE IS TYPICAL UNDERNEATH THE ADJACENT HMAC OR CONCRETE PAVEMENT.
5. A 12 INCH COMPACTED BASE IS TYPICAL UNDERNEATH THE TRACK BED.
6. A 7 FEET NO BORE ZONE EXISTS FROM FINISHED TRACK/ROADWAY TO A DEPTH OF 7 FEET.
7. SUBGRADE DEPTH VARIES THROUGHOUT THE ALIGNMENT.
8. THIRD PARTY UNDERGROUND UTILITIES ARE PRESENT AT VARIOUS SIZES AND DEPTHS.
9. A 7 FEET DEPTH BORING CLEARANCE (FROM FINISHED TRACK/ROADWAY ELEVATION) IS REQUIRED FOR ALL DIRECTIONAL BORING WHILE WITHIN THE OPERATIONAL RIGHT-OF-WAY, SEE FIGURE 7.1.
10. STREETCAR TRACK ASBUILTS ARE AVAILABLE UPON REQUEST FOR SITE SPECIFIC WORK ALONG THE ALIGNMENT, COORDINATE WITH EL PASO STREETCAR.



TYPICAL STREETCAR TRACK BED CROSS - SECTION – SCALE: N.T.S.

FIGURE J - 1



STREETCAR DIRECTIONAL BORE CLEARANCE ZONE

11/20/2019



Exhibit K

Safety Meeting Sign-In Sheet Template



Exhibit L

Track Access Training Card Template





EL PASO
STREETCAR



El Paso Streetcar

El Paso Streetcar Track Access Card

601-A South Santa Fe St.
El Paso, Texas 79901

Name

ID # 20-00XXX

Expiration date: **MO-DAY-YEAR**

Exhibit M

Track Access Program Policy Ordinance



018824

ORDINANCE NO. _____

AN ORDINANCE AMENDING TITLE 13 (STREETS, SIDEWALKS AND PUBLIC PLACES) OF THE EL PASO CITY CODE TO ADD A NEW CHAPTER 13.42 (STREETCAR TRACK ACCESS PROGRAM PROCEDURE) TO ESTABLISH PERMITTING PROCEDURES FOR ANY ACTIVITY OCCURRING WITHIN THE VICINITY OF THE EL PASO STREETCAR INFRASTRUCTURE. THE PENALTY IS AS PROVIDED IN CHAPTER 13.42 OF THE EL PASO CITY CODE.

WHEREAS, the City of El Paso and the Mass Transit Department will begin operations for the El Paso Streetcar, and

WHEREAS, the new permitting procedures for any activity within the vicinity of the El Paso Streetcar are necessary to protect the health, safety, and welfare of the public as well as the integrity of the El Paso Streetcar Infrastructure.

NOW BE IT ORDAINED BY THE EL PASO CITY COUNCIL

Section 1. That Title 13 (Streets, Sidewalks and Public Places) be amended to add a new Chapter 13.42 as follows:

Chapter 13.42- Streetcar Track Access Program Procedure

13.42.010- Definitions.

A. "Activity" means the following:

1. Any construction, demolition, event, moving event, excavation, use of right of way, cleaning, painting, repairing, or installation of traffic control devices, regardless of whether any of the previous requires approval of the City under the El Paso City Code; or
2. Any activity under the El Paso City Code that requires a permit, license, permission, or review from the City.

B. "Streetcar Safety Officer" means the person designated by the City Manager to oversee the safety of all Streetcar Infrastructure and Streetcar operations.

C. "Streetcar" means the rail mass transit system owned and operated by the City of El Paso.

D. "Streetcar Infrastructure" means any component or item used for the operation of the Streetcar, including but not limited to, the railcars, track works, traction power substations, overhead contact systems, overhead contact system poles, train wayside communications systems, and/or the streetcar maintenance facility.

13.42.020- General

018824

ORDINANCE NO. _____

A. Any person or entity performing any activity under any Streetcar Infrastructure or within 10 feet of any Streetcar Infrastructure must comply with the Track Access Program Procedure as adopted and amended by the Streetcar Safety Officer from time to time.

B. The Streetcar Safety Officer has authority to create and revise the Track Access Program Procedure for purposes of safeguarding the public health and safety in accordance with best practices as well as the protection of the Streetcar Infrastructure. The Streetcar Safety Officer shall make the Track Access Program Procedure available to the public.

C. The City Council may establish any fees for administrative costs, track access training, system reconfiguration, and personnel time needed to reconfigure the system. City Council may establish the fees through the annual budget resolution, amendment to the annual budget resolution, or other resolution, ordinance or act of the City Council.

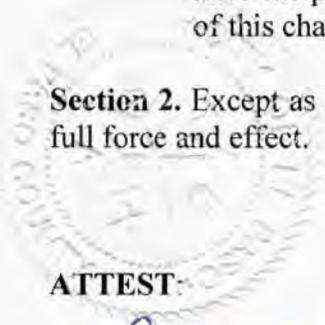
D. For purposes of protecting the health, safety, and welfare of all citizens, the Streetcar Safety Officer is authorized to issue a stop work order to any activity that does not comply with this chapter and/or the Track Access Program Procedure.

13.42.030 – Violation- Penalty.

A. Any person who shall violate any provision of this chapter shall, upon conviction, be guilty of a misdemeanor and punished by a fine not exceeding five hundred dollars. Each day the violation occurs shall constitute a separate offense.

B. The Streetcar Safety Officer, the permit official, and any code enforcement officer of the City of El Paso are authorized to enforce the provisions of this chapter and shall have the power to issue misdemeanor citations to any person violating the provisions of this chapter.

Section 2. Except as herein amended, Title 13 (Streets, Sidewalks and Public Places) remains in full force and effect.



ATTEST:

Laura D. Prine
Laura D. Prine
City Clerk

CITY OF EL PASO:

Dee Margo
Dee Margo
Mayor

APPROVED AS TO FORM:

Omar A. De La Rosa
Omar A. De La Rosa
Assistant City Attorney

APPROVED AS TO CONTENT:

Jay Banasiak
Jay Banasiak, Director
Mass Transit Department

018824

ORDINANCE NO. _____